

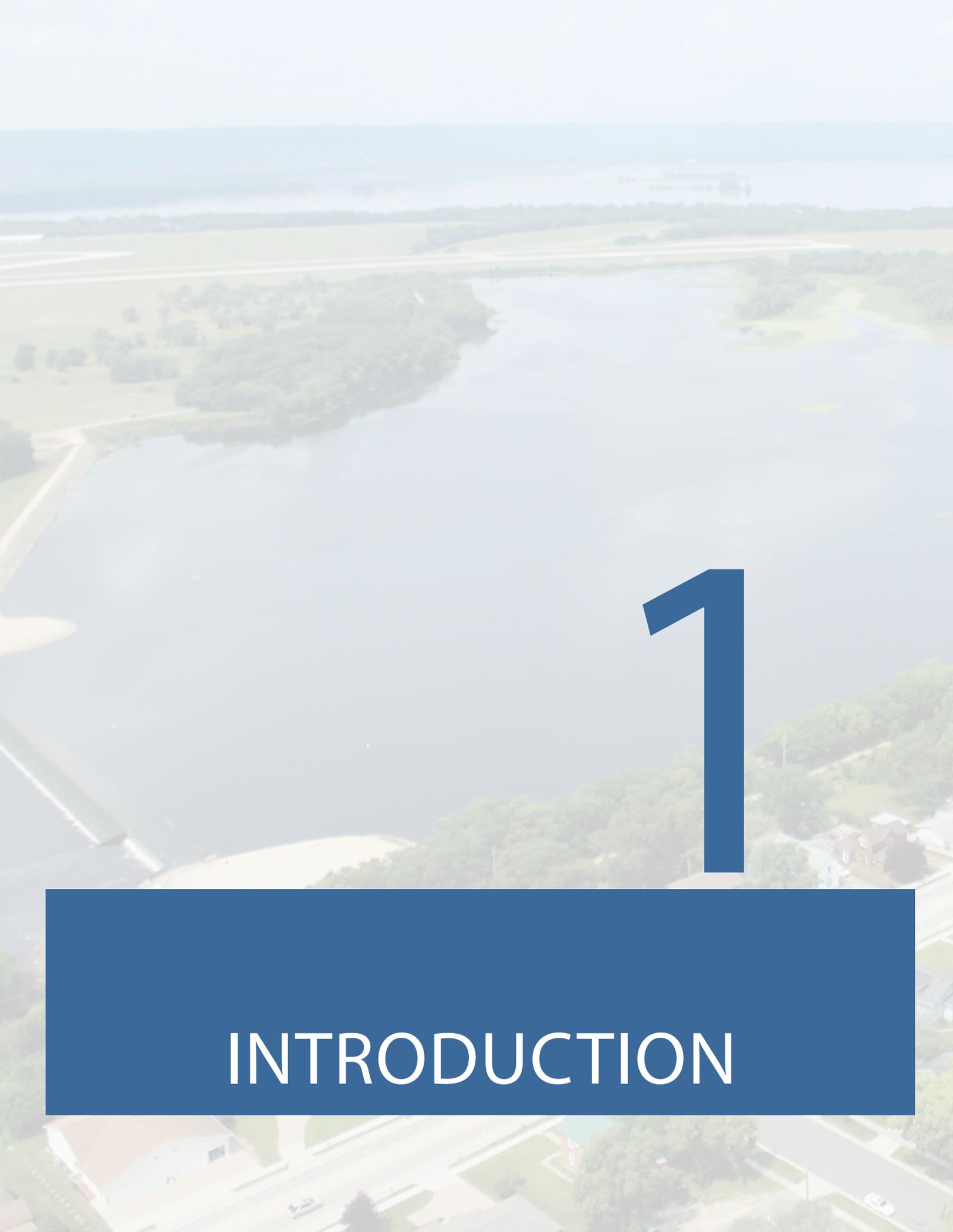


CITY OF ONALASKA

BUILDING THE GREAT RIVER LANDING

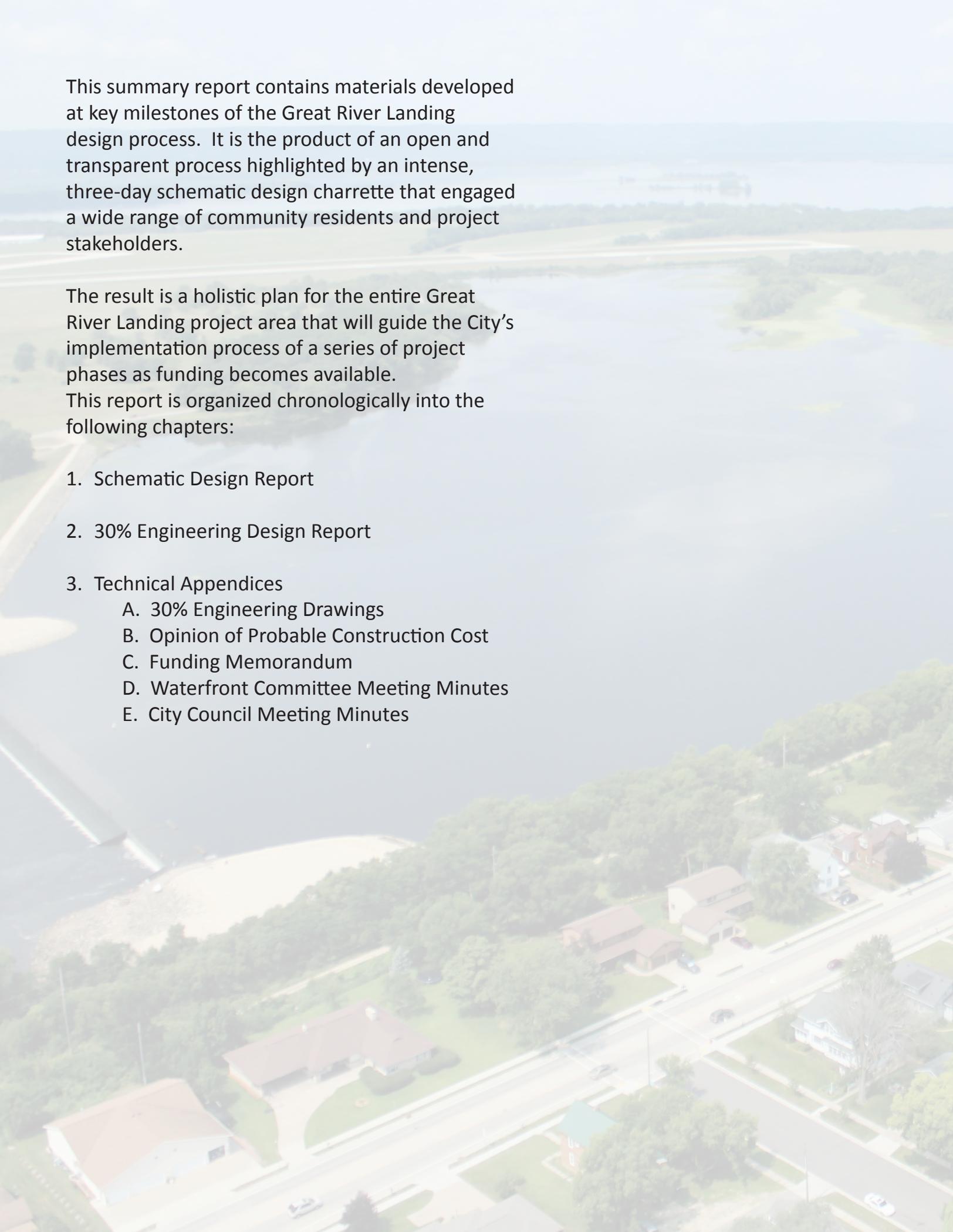
JANUARY 2015



An aerial photograph of a large, calm lake. On the left side, a concrete dam is visible, with a road leading to it. The lake is surrounded by green fields and trees. In the distance, more water and land are visible under a clear sky. In the bottom right corner, a residential neighborhood with houses and streets is partially visible.

1

INTRODUCTION



This summary report contains materials developed at key milestones of the Great River Landing design process. It is the product of an open and transparent process highlighted by an intense, three-day schematic design charrette that engaged a wide range of community residents and project stakeholders.

The result is a holistic plan for the entire Great River Landing project area that will guide the City's implementation process of a series of project phases as funding becomes available.

This report is organized chronologically into the following chapters:

1. Schematic Design Report
2. 30% Engineering Design Report
3. Technical Appendices
 - A. 30% Engineering Drawings
 - B. Opinion of Probable Construction Cost
 - C. Funding Memorandum
 - D. Waterfront Committee Meeting Minutes
 - E. City Council Meeting Minutes

ACKNOWLEDGEMENTS

The City and design team appreciates all of the residents, business owners, elected officials and other project stakeholders who devoted their valuable time to participate in the design process to make it reflective of the community. As such, the plan acknowledges the following individuals and organizations who participated:

City of Onalaska

- Mayor Joe Chilsen
- Common Council
 - Jim Bialecki – Council President
 - Erik Sjolander
 - Jim Olson
 - Bob Muth
 - Harvey Bertrand
 - Jack Pogreba
- Great River Landing Waterfront Committee
 - Erik Sjolander, Alderperson – Chair
 - John Burnett – Vice Chair
 - Harvey Bertrand, Alderperson
 - Gary Lass
 - Mary Cody
 - Dustin Hundt
 - Debbie Clarkin
- Community Development Authority
 - Mike Gargaro – Chair
 - John Lyche – Vice Chair
 - Jim Bialecki, Alderperson
 - Jim Olson, Alderperson
 - Barry Blomquist
 - Ann Brandau
 - Mark Hansen
- Planning & Zoning Department
 - Brea Grace, AICP, Land Use & Development Director
 - Katie Meyer, AICP, Planner / Zoning Inspector
 - Joe Barstow, GIS Technician
- Engineering Department
 - Jarrod Holter, PE, City Engineer
- Parks and Recreation Department
 - Dan Wick, Parks & Recreation Director
- Mark Hansen, Park Maintenance Supervisor

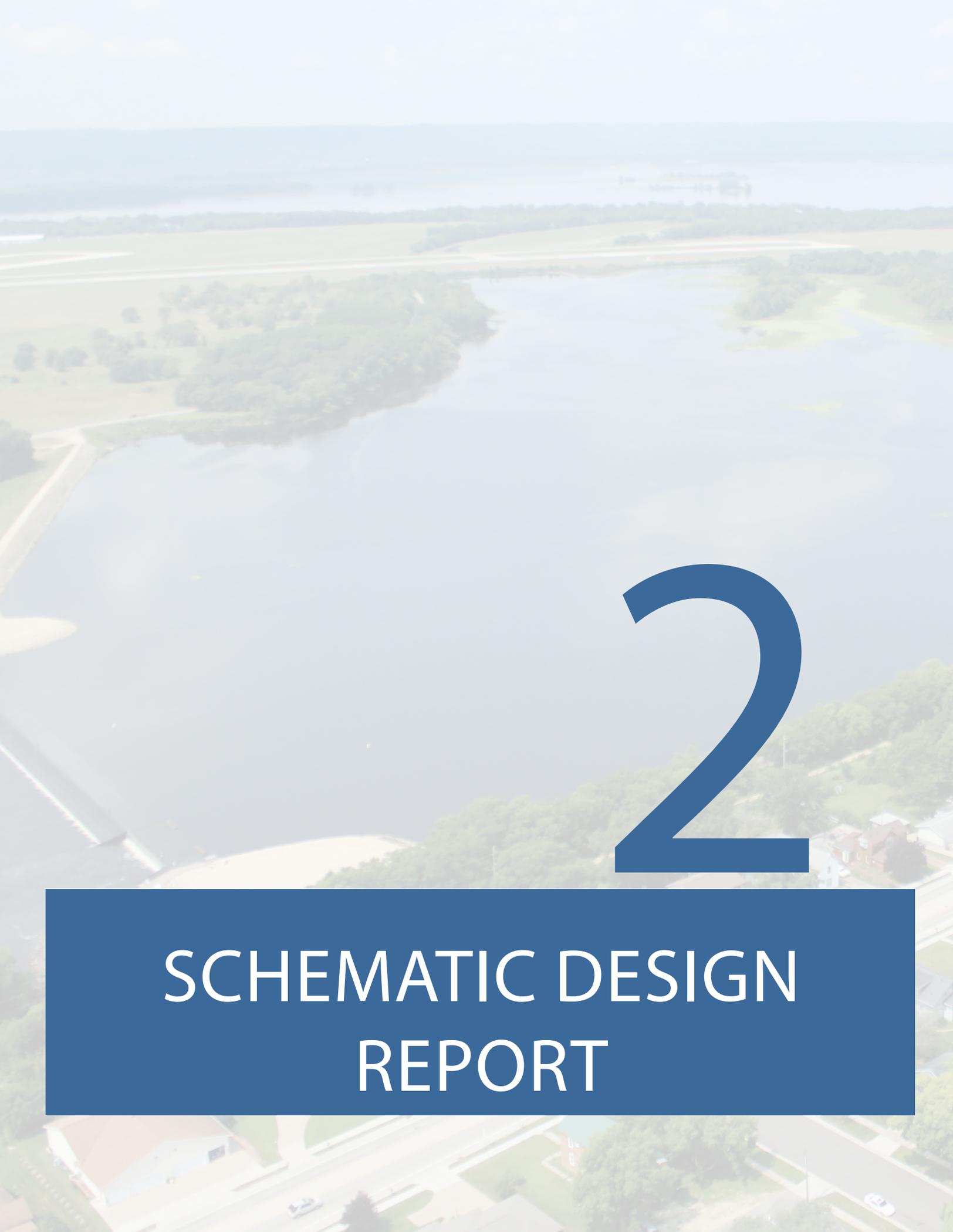
Project Stakeholders

- U.S. Fish & Wildlife Service
- U.S. Army Corps of Engineers
- Burlington Northern Santa Fe Railway
- Wisconsin Department of Natural Resources
- Mississippi Valley Archaeology Center
- Onalaska School District
- Luther High School
- Three Rivers Roleo
- Ona Bike
- Wisconsin Bike Fed
- La Crosse County Snowmobile Alliance
- Coulee Sno-Drifters
- North American Squirrel Association
- 7 Rivers Region Outdoor Recreation Alliance
- Coulee Audubon Society

Consultant Team

- Short Elliott Hendrickson Inc. (SEH)
- Abonmarche

A special thank you to Mary Cody, Mary Cody's Restaurant & Catering and to Sean McAlister, Holiday Inn Express for food and lodging accommodations during the charrette.

An aerial photograph of a large, calm lake. On the left side, a concrete dam structure is visible, extending into the water. The surrounding landscape includes green fields, dense trees, and a residential neighborhood with houses and roads at the bottom of the frame. The sky is clear and blue.

2

SCHEMATIC DESIGN REPORT

Building the Great River Landing

Schematic Design Report

November 3, 2014

The City of Onalaska's Waterfront Committee has undertaken the goal of revitalizing the Onalaska waterfront and Building the Great River Landing to provide safe and accessible access to the waterfront with clear connectivity to the downtown and Main Street.

From August through October, the City hosted a Community Meeting and a three day Charrette or intensive collaborative design and planning session (Sept 29 - Oct 1), culminating in this schematic design for Onalaska's waterfront.

During the several months that follow, the City's consultant team, along with the Waterfront Committee, will advance the design through preliminary engineering and cost estimation.

For more information:
www.greatriverlanding.com



PROCESS

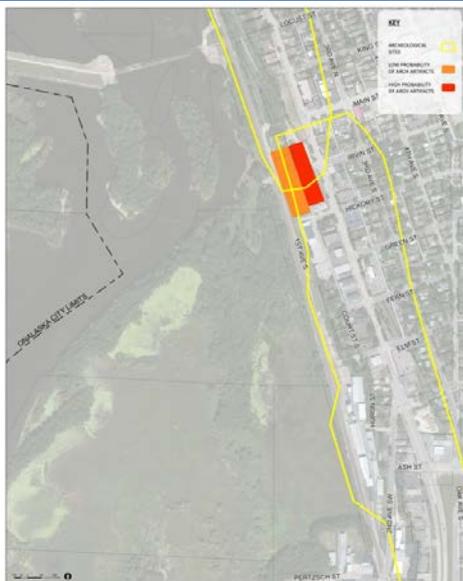


On August 20, 2014, the City of Onalaska's Waterfront Committee hosted a Community Meeting as a first in the series of public involvement opportunities regarding the Onalaska waterfront and the Great River Landing.



From that meeting until October, the City and design team held stakeholder interviews and a three day Charrette or intensive collaborative design and planning session (Sept 29 - Oct 1), culminating in this schematic design for Onalaska's waterfront.

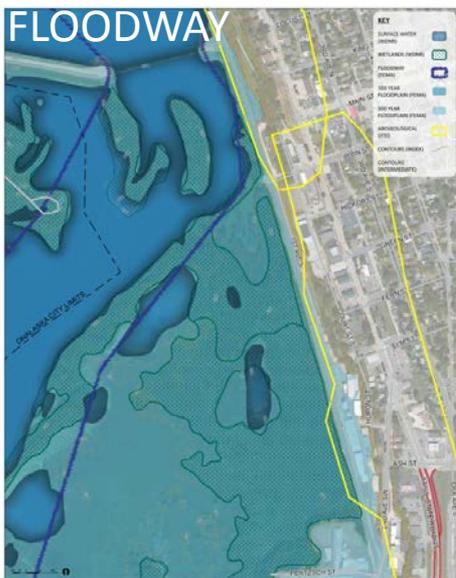




ENVIRONMENT
Building the Great River Landing



SOILS
Building the Great River Landing



ENVIRONMENTAL BASEMAP
Building the Great River Landing

The design team met with City staff and stakeholders and reviewed various ecological, hydraulic, cultural and historic documents throughout prior to and during the design charrette to develop a better understanding of the site's characteristics and potential for development.

The project site is essentially divided by the BNSF railroad, with the waterfront bottomlands to the west and the City's downtown to the east. The railroad provides significant challenges for safe access to the site. BNSF representatives were contacted and interviewed during the process and share similar goals of the City for safe and controlled access to both portions of the site.

Floodways and wetlands provide excellent recreational opportunities and construction challenges. Various agencies including DNR, USFWS and USACE were consulted prior to and throughout the charrette process.

Archaeological findings are also throughout the project area and the design team recognized these areas as interpretive opportunities and construction challenges.

All of these site factors were carefully considered when developing the plans for the project.

PROJECT GOALS & GUIDING PRINCIPLES

PROJECT GOALS

- A. **A highly accessible river**
- B. **An active riverfront connected to a vibrant downtown**
- C. **A design in harmony with nature & ecology**
- D. **Support and enhance education and awareness of history, culture and environment**
- E. **Balance creative vision and market reality**
- F. **Community supported policies, plans and projects**

GUIDING PRINCIPLES

- 1. **Create a Destination**
- 2. **Provide a Range of Uses and Activities**
- 3. **Enhance & Promote Riverfront Environment & Ecology**
- 4. **Economically Viable and Sustainable Return on Investment**
- 5. **Increase and Enhance Connections**
- 6. **Address and Enhance Public Safety and Security**
- 7. **Respect and Promote Heritage & Culture**
- 8. **Embrace Resiliency**
- 9. **Ongoing Community Engagement**



THE BASICS

EMS ACCESS

- BOARDWALK: SPILLWAY TO BEACH/PLATFORMS
- NATURE TRAILS
- PROBLE BASIN & LOADING
- LOWLANDS MINIMAL SUPPORT STRUCTURE
- OUTDOOR CLASSROOM (COUNCIL RING)
- WILDLIFE/HABITAT/INSTEAD/CULTURAL/HISTORIC

ROOSTROOM FACILITY

PARKING/TRAILHEAD

PUBLIC SQUARE: MARKET

OVERLOOKS

VEG. CLEARING FOR VIEWS

MIXED USE DEVELOPMENT

BRIDGE

CIVIC FEATURE

OPTIONS

	A MAIN EXTENDED	B BLACK RIVER ENGAGED
LOW	ACTIVE UPLAND	SOFTER UPLANDS
	QUIET LOWLANDS - NO LIGHTS	ACTIVE LOWLANDS
	POD BRIDGE/OVERLOOK - OFF MAIN	FUNCTIONAL POD BRIDGE
	MAIN ST. PLAZA - MARKET	RUIN BRIDGE - NO DE GRASS
	TRAILHEAD & PARKING	PLAY/ZIP
	FOCAL POINT ON MAIN	SPILLWAY CROSSING
	TEACHING/PROBLE	TRANSITION DOCKS
	NATURE TRAILS MARSH - SOFT/CANAL RAIN	FLEX LUNGS/PARKING UPLANDS
UP	BANK CASUALTY LEARN AND WATER SEEK	
	BIRD REHABILITATION CENTER	

Following the development of the Goals and Principles and preliminary program ideas presented to the public on September 29, 2014, the design team developed the consensus program as shown on the bottom left as the “Basics”. The “Options” were developed through the community’s input for the design team’s plan alternatives, as shown above. These were presented to the Waterfront Committee on September 30, and the design team received feedback to proceed with Option A on the top left.

PRELIMINARY SCHEMATIC PLAN



The plan graphic to the left represents the consensus schematic design plan that was developed during the final day of the charrette (October 1, 2014). The list below corresponds to the consensus program that was developed for the project and keyed into the plan graphic.

Project Program Elements

1. Transient Boat Docking
2. Ice Fishing Bay
3. Spillway Island Bridging
4. Nature Walk & Rustic Camping
5. Aerial Canopy Course
6. Spillway Fishing Pier
7. Pedestrian Bridge with Grand Stairway & Elevator
8. Main Street Plaza
9. Market, Trailhead & Restroom Facility
10. Enhanced Pedestrian & Emergency Railroad Crossing
11. Emergency Boat Launch, ADA Kayak Launch & Log Rolling Boom
12. Stone Seating Blocks
13. Adventure Play Area
14. Support Structure
15. Fabric Picnic Shelters
16. Stone River Access
17. Wetland Viewing Access
18. Nature Walk

GREAT RIVER LANDING

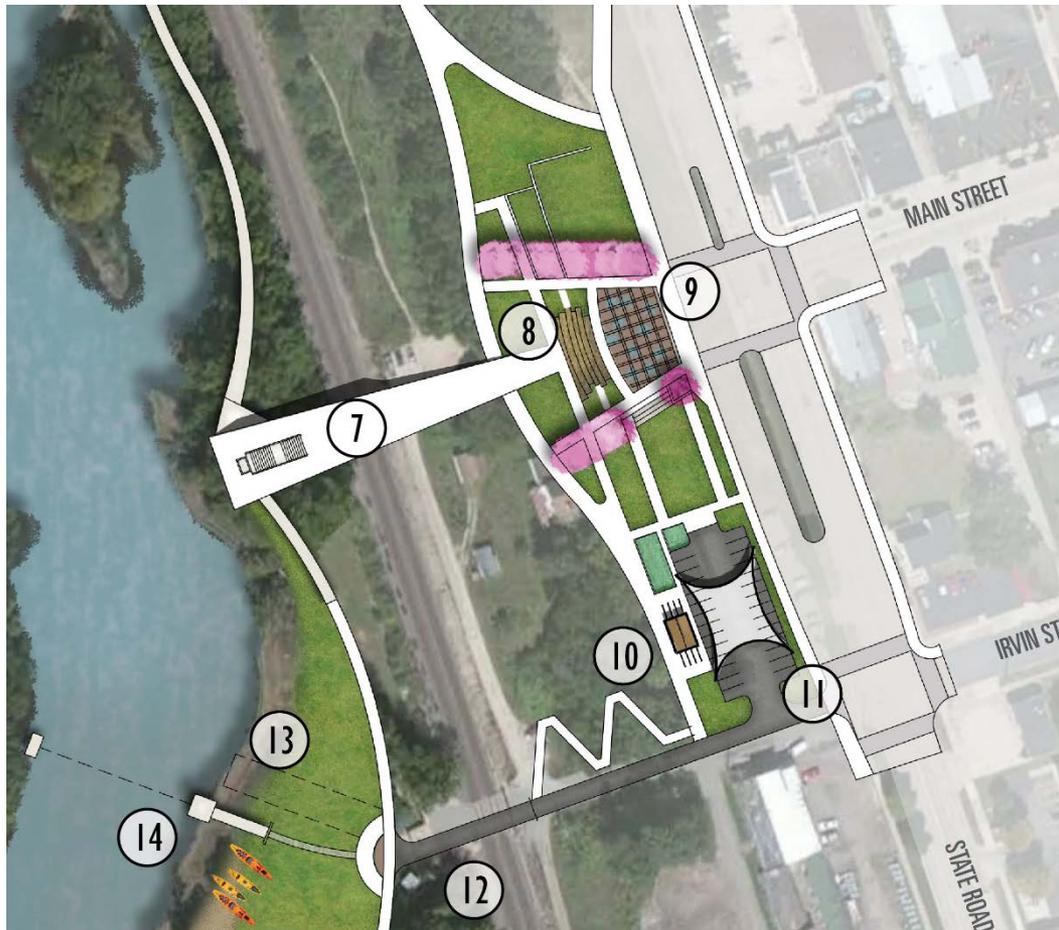


The consensus plan graphic to the left provides additional detail for the development zone of the project. The design concept is for an extended Main Street to be directly connected to the waterfront. The design team provided this through a large pedestrian bridge and overlook that would span the railroad tracks, provide safe and accessible connection to the waterfront as a direct extension of Main Street. Additional details of the upland portion can be found on the next page.

The waterfront has been designed for both passive and active recreation. The design includes a nature playground, picnic grove, log rolling boom and kayak launch along with other informal water's edge access. Shown below is a character rendering of the boardwalk system proposed to connect the active area to adjacent to the spillway.



THE LANDING - MAIN STREET PLAZA & BRIDGE



7. Pedestrian Bridge with Grand Stairway & Elevator
8. Amphitheater Seating
9. Main Street Plaza with Interactive Water Feature
10. Restroom Facilities
11. Trailhead Parking & Market
12. Enhanced Pedestrian & Emergency Railroad Crossing
13. Emergency Boat Launch,
14. ADA Kayak Launch & Log Rolling Boom

The uplands portion of the project includes a trailhead with parking and restroom facilities (that could double as a farmer's market), plaza with an interactive water feature adjacent to Main Street and a grand, accessible amphitheater steps for passive relaxation and enjoying views of the waterfront. A pedestrian bridge provides access to the waterfront as a direct extension of Main Street, to capitalize on the City's investments in the Downtown. The project will be designed to meet and exceed ADA minimum standards for accessibility. Below are character renderings of what this site could be.



ACTION PLAN – PHASING ALTERNATIVES



The plan graphic to the left shows potential phasing and bundling projects that could assist implementing the Great River Landing in a phased approach as funding is available. Below are brief descriptions of these bundles along with the Engineer's Preliminary Opinion of Construction Cost for each. Design, engineering and permitting fees are not included in the estimates below. These bundles are arbitrarily numbered and do not reflect City priorities.

1. Paddle Basin, Nature Playground & Trails (\$2.5M)
2. Boardwalk & Spillway Link (\$2.3M)
3. South Nature Trail Enhancements (\$525,000)
4. Spillway Bridges (\$3.6M)
5. Adventure Play & Island Recreation (\$700,000)
6. Trailhead & Market (\$950,000)
7. Main Street Bridge (\$3.2M)
8. Main Street Plaza, Spray Fountain & Lawn (\$1.5M)

Total Project Construction = \$15.3M

Thank You



An aerial photograph of a large, calm lake. On the left side, a concrete dam structure is visible, extending into the water. The surrounding landscape includes green fields, dense trees, and a residential neighborhood with houses and roads at the bottom of the frame. The sky is clear and blue.

3

**30% ENGINEERING
DESIGN REPORT**

Building the Great River Landing

30% Design Briefing

Parks and Recreation Board - January 26, 2015



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PROCESS



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PROJECT GOALS & GUIDING PRINCIPLES

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GUIDING PRINCIPLES

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- 3. Enhance & Promote Riverfront Environment & Ecology**
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- 5. Increase and Enhance Connections**
- 6. Address and Enhance Public Safety and Security**
- 7. Respect and Promote Heritage & Culture**
- 8. Embrace Resiliency**
- 9. Ongoing Community Engagement**



GREAT RIVER LANDING – SCHEMATIC DESIGN



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Project Program Elements

1. Transient Boat Docking
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6. Spillway Fishing Pier
7. Pedestrian Bridge with Grand Stairway & Elevator
8. Amphitheater Seating
9. Main Street Plaza
10. Restroom Facility
11. Trailhead & Parking
12. Enhanced Pedestrian & Emergency Railroad Crossing
13. Emergency Boat Launch,
14. ADA Kayak Launch & Log Rolling Boom
15. Stone Seating Blocks
16. Adventure Play Area
17. Support Structure
18. Fabric Picnic Shelters
19. Stone River Access
20. Wetland Viewing Access
21. Nature Walk

LEGEND					
1	TRANSIENT BOAT DOCKING	8	AMPHITHEATER SEATING	15	STONE SEATING BLOCKS
2	ICE FISHING BAY	9	PLAZA WITH INTERACTIVE WATER FEATURE	16	ADVENTURE PLAY AREA
3	SPILLWAY ISLAND BRIDGING	10	RESTROOM FACILITY	17	SUPPORT STRUCTURE
4	NATURE WALK & RUSTIC CAMPING	11	TRAILHEAD & PARKING	18	FABRIC PICNIC SHELTERS
5	AERIAL CANOPY COURSE	12	ENHANCED PEDESTRIAN & EMERGENCY R.R. CROSSING	19	STONE RIVER ACCESS
6	SPILLWAY FISHING PIER	13	EMERGENCY BOAT LAUNCH	20	WETLAND VIEWING ACCESS
7	PEDESTRIAN BRIDGE WITH GRAND STAIRWAY	14	KAYAK LAUNCH & LOG ROLLING BOOM	21	NATURE WALK

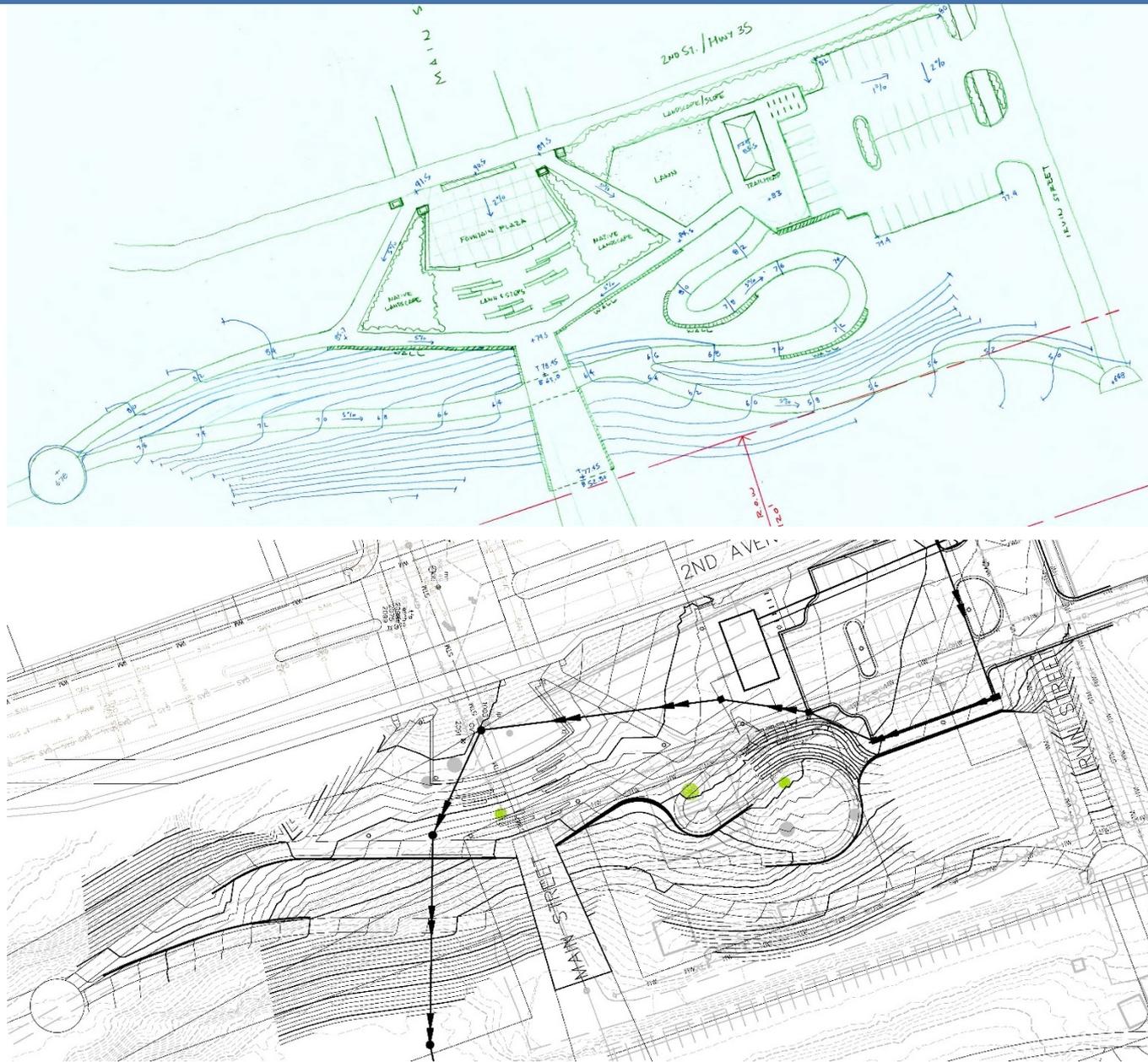
30% DESIGN PLANS – SCOPE REVIEW



SEH has advanced the following areas, as noted below, to assist the City in refining the cost, design and engineering of these projects.

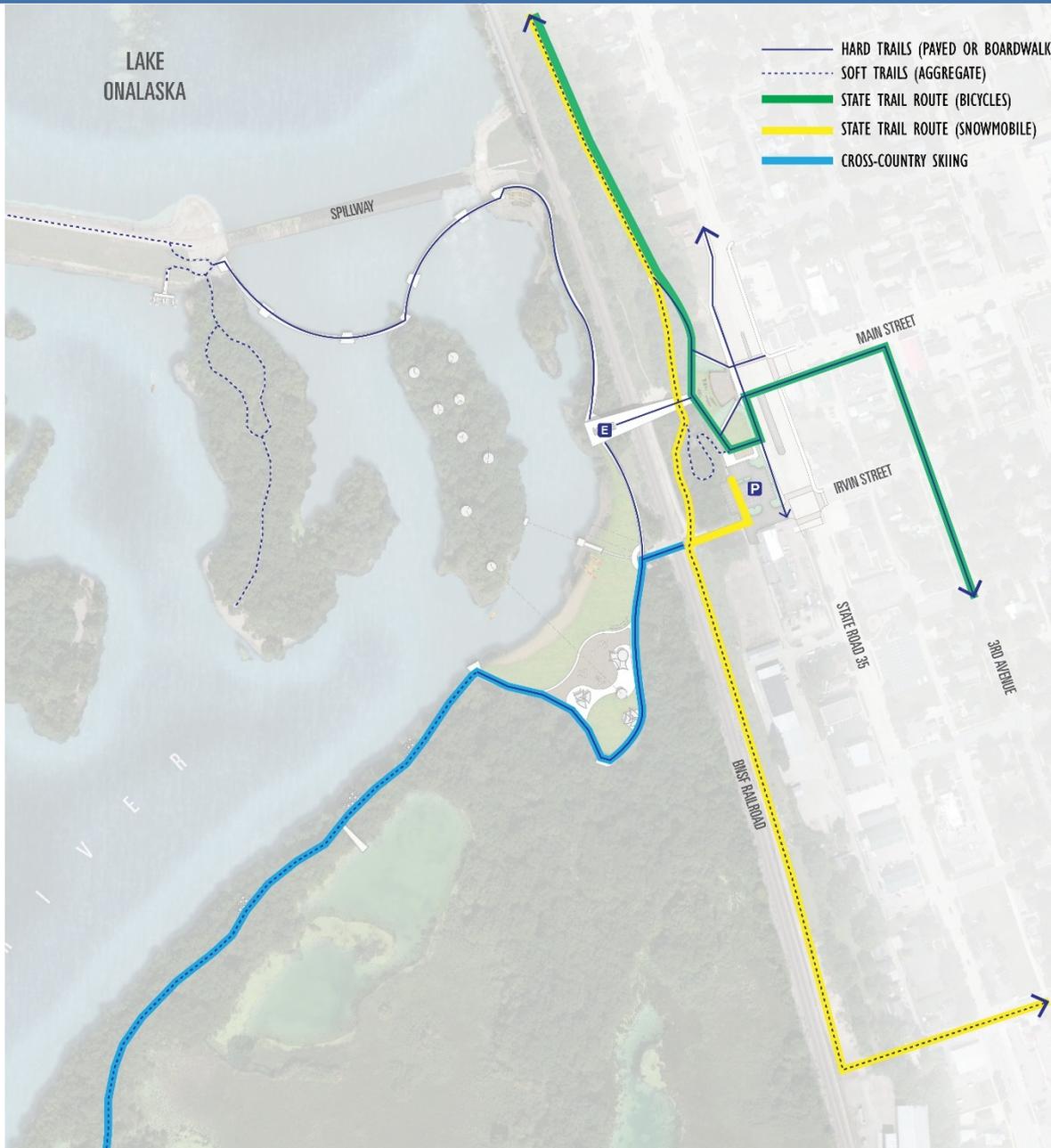
- 1. Paddle Basin, Nature Playground & Trails**
15% review of bridge landing
- 2. Boardwalk & Spillway Link**
10% review of bridge landing
- 3. South Nature Trail Enhancements**
Verify costs
- 4. Spillway Bridges**
Verify costs
- 5. Adventure Play & Island Recreation**
Verify costs
- 6. Trailhead & Market**
 1. 35% grading & layout plan
 2. 10% architectural design for trailhead building
- 7. Main Street Bridge**
 1. 5% structural review of pier/stair/elevator
 2. Bridge width options
- 8. Main Street Plaza, Spray Fountain & Lawn**
25% grading & layout plan

THE LANDING – GRADING PLAN



SEH addressed the upland site grading early in the 30% process, to validate the design generated in the charrette. To provide Universal Accessibility (5% maximum slopes) and minimize earthwork and impacts to the bluff, SEH developed the grading plans shown to the left.

THE LANDING – CIRCULATION PLAN



SEH reviewed and refined the circulation patterns of multiple trail types and users early in the process to eliminate any redundancies from the charrette plan. The graphic to the left indicates proposed circulation patterns within and through the site for pedestrians, bicycles, snowmobiles and cross-country skiing.

GREAT RIVER LANDING – UPDATED DESIGN



The plan graphic to the left represents the updates to the schematic design plan that was developed during the final day of the charrette. The consensus program listed below still remains intact, while some of the geometry and materials have slightly changed based upon grading, circulation, City input and actual site conditions.

Project Program Elements

1. Transient Boat Docking
2. Ice Fishing Bay
3. Spillway Island Bridging
4. Nature Walk & Rustic Camping
5. Aerial Canopy Course
6. Spillway Fishing Pier
7. Pedestrian Bridge with Grand Stairway & Elevator
8. Amphitheater Seating
9. Main Street Plaza
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13. Emergency Boat Launch,
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15. Stone Seating Blocks
16. Adventure Play Area
17. Support Structure
18. Fabric Picnic Shelters
19. Stone River Access
20. Wetland Viewing Access
21. Nature Walk

LEGEND

- | | | | | | |
|---|---------------------------------------|----|---|----|------------------------|
| 1 | TRANSIENT BOAT DOCKING | 8 | AMPHITHEATER SEATING | 15 | STONE SEATING BLOCKS |
| 2 | ICE FISHING BAY | 9 | PLAZA WITH INTERACTIVE WATER FEATURE | 16 | ADVENTURE PLAY AREA |
| 3 | SPILLWAY ISLAND BRIDGING | 10 | RESTROOM FACILITY | 17 | SUPPORT STRUCTURE |
| 4 | NATURE WALK & RUSTIC CAMPING | 11 | TRAILHEAD & PARKING | 18 | FABRIC PICNIC SHELTERS |
| 5 | AERIAL CANOPY COURSE | 12 | ENHANCED PEDESTRIAN & EMERGENCY R.R. CROSSING | 19 | STONE RIVER ACCESS |
| 6 | SPILLWAY FISHING PIER | 13 | EMERGENCY BOAT LAUNCH | 20 | WETLAND VIEWING ACCESS |
| 7 | PEDESTRIAN BRIDGE WITH GRAND STAIRWAY | 14 | KAYAK LAUNCH & LOG ROLLING BOOM | 21 | NATURE WALK |

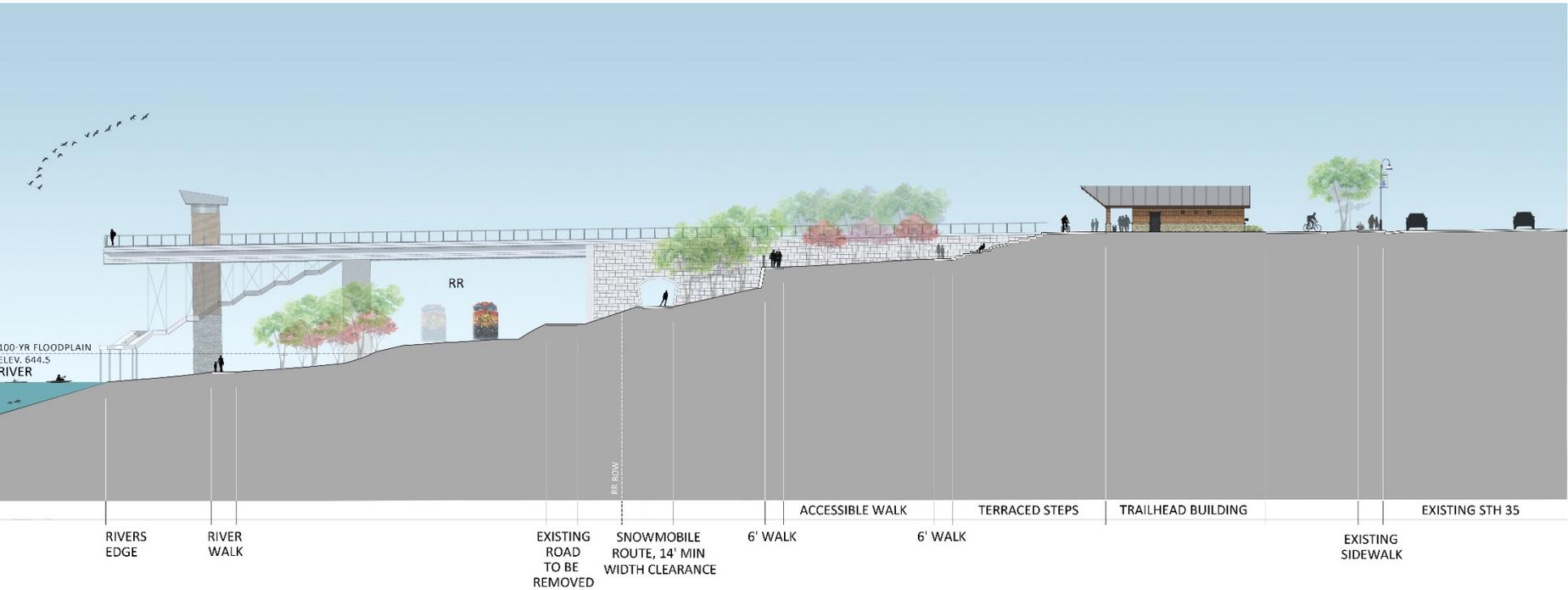


THE LANDING - MAIN STREET PLAZA & BRIDGE



7. Pedestrian Bridge with Grand Stairway & Elevator
8. Amphitheater Seating
9. Main Street Plaza with Interactive Water Feature
10. Restroom Facilities
11. Trailhead Parking & Market
12. Enhanced Pedestrian & Emergency Railroad Crossing
13. Emergency Boat Launch
14. ADA Kayak Launch & Log Rolling Boom

THE LANDING - MAIN STREET PLAZA & BRIDGE



THE LANDING – PROGRAMMING PLAN

The plan to the left shows different programming activities throughout the Great River Landing that could be operated by City staff or outside organizations.

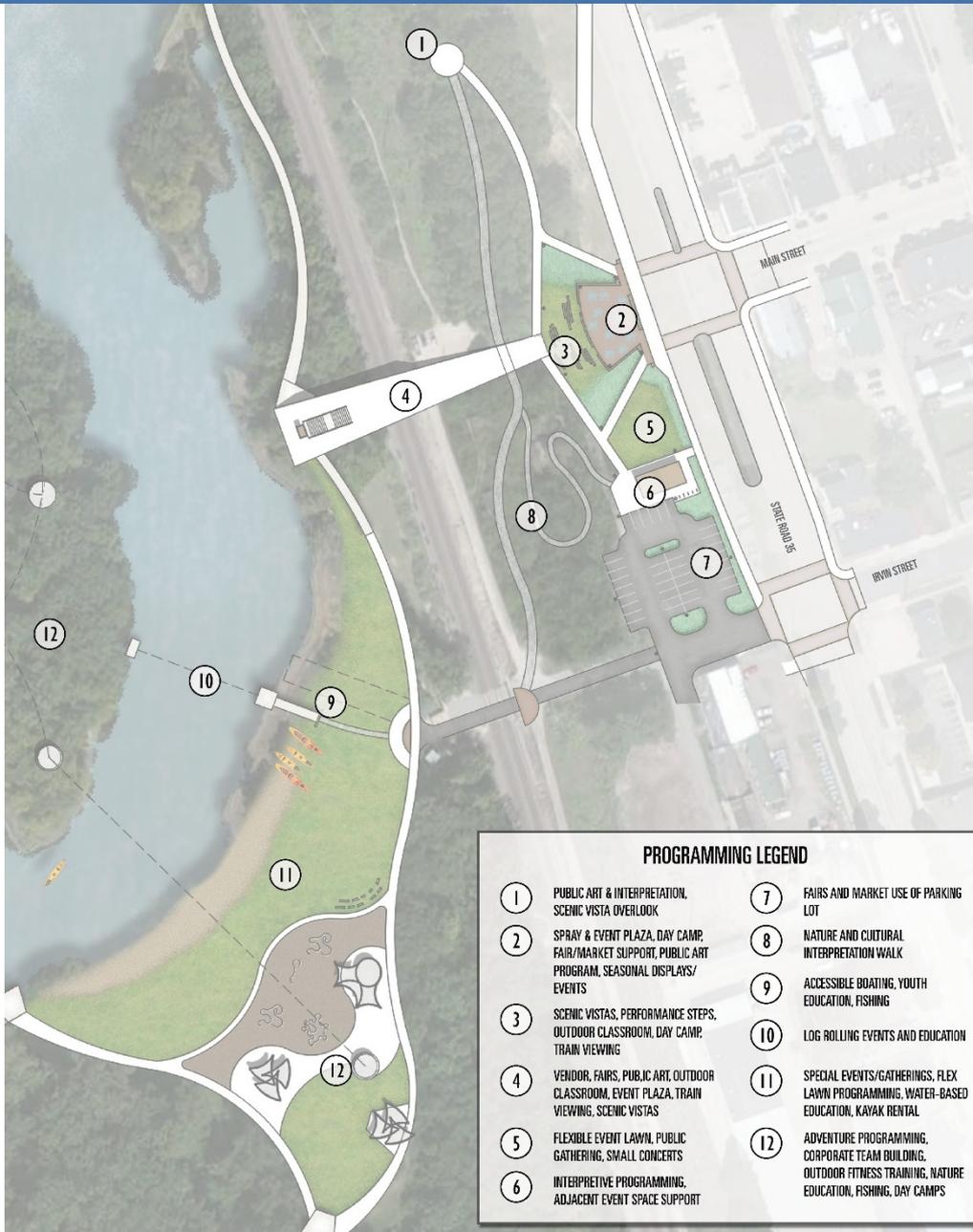
1. TRANSIENT BOATING, WATER-BASED EDUCATION, FISHING
2. DAY/OVERNIGHT CAMPING, NATURE EDUCATION, FISHING
3. YOUTH FISHING, NATURE INTERPRETATION, DAY CAMPS
4. ADVENTURE PROGRAMMING, CORPORATE TEAM BUILDING, OUTDOOR FITNESS TRAINING, NATURE EDUCATION, FISHING, DAY CAMPS
5. ICE FISHING
6. NATURE INTERPRETATION, OUTDOOR CLASSROOM, DAY CAMPS
7. NATURE INTERPRETATION, HANDS-ON ENVIRONMENTAL EDUCATION, USFWS PROGRAMMING, ICE SKATING
8. SPECIAL EVENTS/GATHERINGS, FLEX LAWN PROGRAMMING, WATER-BASED EDUCATION, KAYAK RENTAL

PROGRAMMING LEGEND

- | | | | |
|---|--|---|---|
| 1 | TRANSIENT BOATING, WATER-BASED EDUCATION, FISHING | 5 | ICE FISHING |
| 2 | DAY/OVERNIGHT CAMPING, NATURE EDUCATION, FISHING | 6 | NATURE INTERPRETATION, OUTDOOR CLASSROOM, DAY CAMPS |
| 3 | YOUTH FISHING, NATURE INTERPRETATION, DAY CAMPS | 7 | NATURE INTERPRETATION, HANDS-ON ENVIRONMENTAL EDUCATION, USFWS PROGRAMMING, ICE SKATING |
| 4 | ADVENTURE PROGRAMMING, CORPORATE TEAM BUILDING, OUTDOOR FITNESS TRAINING, NATURE EDUCATION, FISHING, DAY CAMPS | 8 | SPECIAL EVENTS/GATHERINGS, FLEX LAWN PROGRAMMING, WATER-BASED EDUCATION, KAYAK RENTAL |



THE LANDING – PROGRAMMING PLAN



The plan to the left shows different programming activities throughout the Great River Landing that could be operated by City staff or outside organizations.

1. PUBLIC ART & INTERPRETATION, SCENIC VISTA OVERLOOK
2. SPRAY & EVENT PLAZA, DAY CAMP, FAIR/MARKET SUPPORT, PUBLIC ART PROGRAM, SEASONAL DISPLAYS/EVENTS
3. SCENIC VISTAS, PERFORMANCE STEPS, OUTDOOR CLASSROOM, DAY CAMP, TRAIN VIEWING
4. VENDOR, FAIRS, PUBLIC ART, OUTDOOR CLASSROOM, EVENT PLAZA, TRAIN VIEWING, SCENIC VISTAS
5. FLEXIBLE EVENT LAWN, PUBLIC GATHERING, SMALL CONCERTS
6. INTERPRETIVE PROGRAMMING, ADJACENT EVENT SPACE SUPPORT
7. FAIRS AND MARKET USE OF PARKING LOT
8. NATURE AND CULTURAL INTERPRETATION WALK
9. ACCESSIBLE BOATING, YOUTH EDUCATION, FISHING
10. LOG ROLLING EVENTS AND EDUCATION
11. SPECIAL EVENTS/GATHERINGS, FLEX LAWN PROGRAMMING, WATER-BASED EDUCATION, KAYAK RENTAL
12. ADVENTURE PROGRAMMING, CORPORATE TEAM BUILDING, OUTDOOR FITNESS TRAINING, NATURE EDUCATION, FISHING, DAY CAMPS

PROGRAMMING LEGEND

- | | |
|--|--|
| ① PUBLIC ART & INTERPRETATION, SCENIC VISTA OVERLOOK | ⑦ FAIRS AND MARKET USE OF PARKING LOT |
| ② SPRAY & EVENT PLAZA, DAY CAMP, FAIR/MARKET SUPPORT, PUBLIC ART PROGRAM, SEASONAL DISPLAYS/EVENTS | ⑧ NATURE AND CULTURAL INTERPRETATION WALK |
| ③ SCENIC VISTAS, PERFORMANCE STEPS, OUTDOOR CLASSROOM, DAY CAMP, TRAIN VIEWING | ⑨ ACCESSIBLE BOATING, YOUTH EDUCATION, FISHING |
| ④ VENDOR, FAIRS, PUBLIC ART, OUTDOOR CLASSROOM, EVENT PLAZA, TRAIN VIEWING, SCENIC VISTAS | ⑩ LOG ROLLING EVENTS AND EDUCATION |
| ⑤ FLEXIBLE EVENT LAWN, PUBLIC GATHERING, SMALL CONCERTS | ⑪ SPECIAL EVENTS/GATHERINGS, FLEX LAWN PROGRAMMING, WATER-BASED EDUCATION, KAYAK RENTAL |
| ⑥ INTERPRETIVE PROGRAMMING, ADJACENT EVENT SPACE SUPPORT | ⑫ ADVENTURE PROGRAMMING, CORPORATE TEAM BUILDING, OUTDOOR FITNESS TRAINING, NATURE EDUCATION, FISHING, DAY CAMPS |



THE LANDING – WATER FEATURE



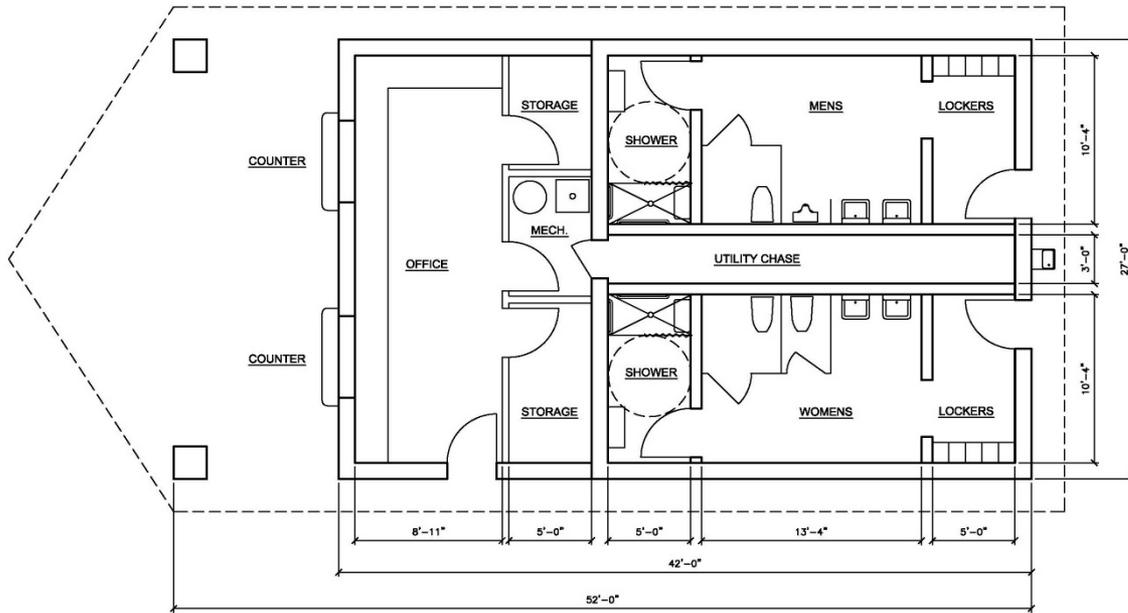
Flow-Through (Potable Water Systems)



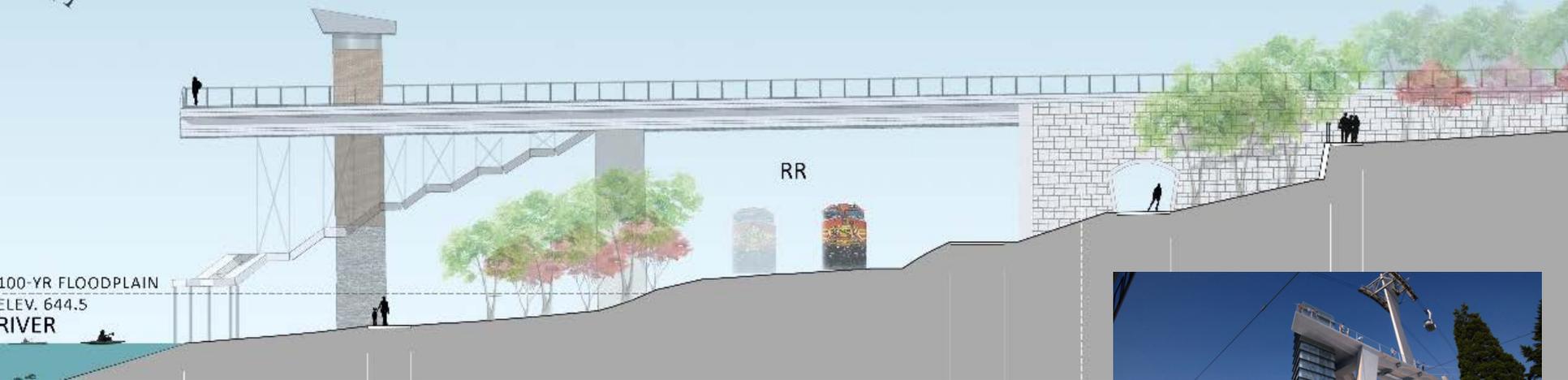
THE LANDING – AMPHITHEATER SEATING



THE LANDING – TRAILHEAD BUILDING



THE LANDING – BRIDGE



Four different bridge options were reviewed by SEH during this phase of the project.

Option A

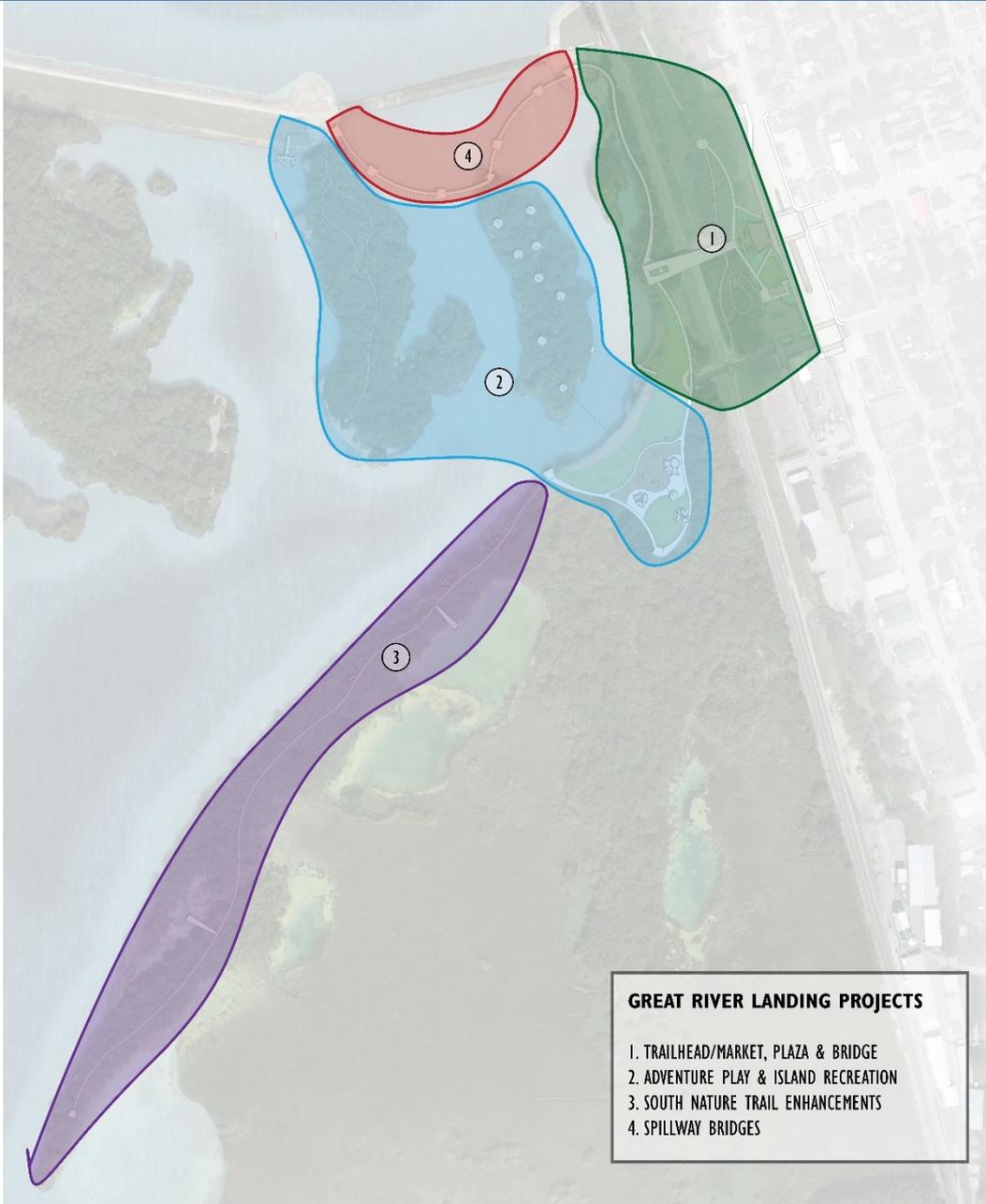
- Bridge per charrette design
- 160' railroad span
- 12" bridge deck & 6' girders
- All concrete, low maintenance
- Increase in earthwork/fill due to higher bridge for railroad clearance

Option B

- Similar to Option A
- Shorter span 125', with extra pier (as shown above)
- Additional pier would be within railroad R.O.W., but outside clear zone
- Precast concrete or steel girders
- Steel girders would result in higher maintenance than concrete
- 6% less than Option A



ACTION PLAN – PHASING ALTERNATIVES CONSTRUCTION COSTS



- GREAT RIVER LANDING PROJECTS**
- 1. TRAILHEAD/MARKET, PLAZA & BRIDGE
 - 2. ADVENTURE PLAY & ISLAND RECREATION
 - 3. SOUTH NATURE TRAIL ENHANCEMENTS
 - 4. SPILLWAY BRIDGES

The plan graphic to the left shows potential phasing and bundling projects that could assist implementing the Great River Landing in a phased approach as funding is available. These bundles are arbitrarily numbered and do not reflect City priorities.

Below are brief descriptions of these bundles along with the Engineer’s Preliminary Opinion of Construction Cost for each. Design, engineering and permitting fees are not included in the estimates below.

Great River Landing Projects

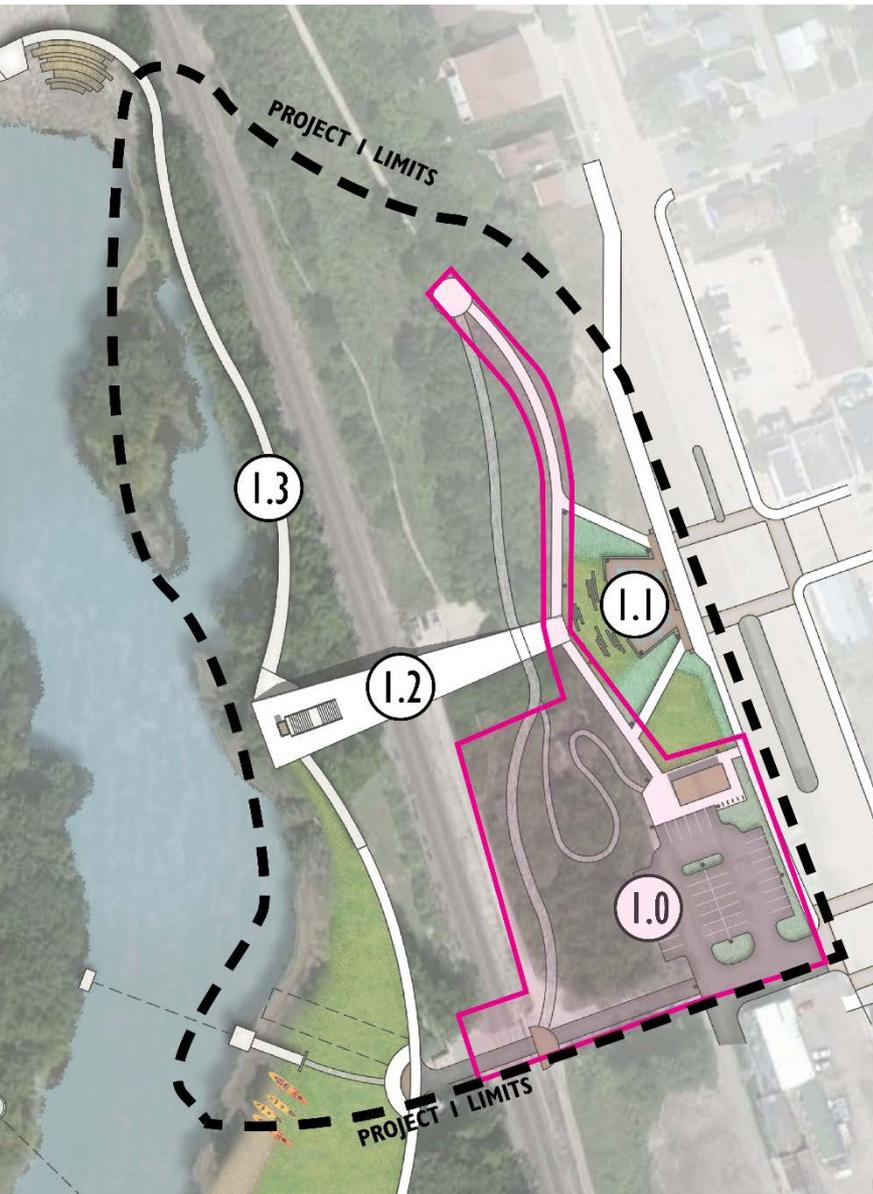
- 1. Trailhead/Market, Plaza & Bridge (\$8.2M)
- 2. Adventure Play & Island Recreation (\$3.1M)
- 3. South Nature Trail Enhancements (\$500,000)
- 4. Spillway Bridges (\$3.4M)

Total Project Construction = \$15.2M

Detailing phasing has only been developed for Project #1 within this contract and can be found on the following page.



ACTION PLAN – PROJECT 1



The plan graphic to the left shows the proposed limits of Project #1 associated with building the Great River Landing. Through phasing and implementation options listed below, a budget range has been developed for Project #1. SEH estimates that Project #1 could be completed within 3-5 years.

- **High = \$8.2M; Low = \$5.0M**

Highlighted in magenta are the approximate limits of the 1st Phase of construction, including the trailhead building and parking, ADA accessible route to the railroad tracks and connection to the Great River State Trail.

1.0: Trailhead/Market = \$1.2M

Below are brief descriptions of alternatives to consider during the final design phase of Project #1, numbered on the plan graphic to the left. These items are phasing or value engineering options for the City to consider as funding is available.

1.1: Plaza (Phasing Options)

1. Sloped Lawn = \$1.1M
2. Final Plaza = \$1.8M

1.2: Bridge (Value Engineering Options)

1. Option D = \$2.2M
2. Per Design = \$3M

1.3: Spillway Trail Connection (Phasing Options)

1. At Grade Trail = \$0.5M
2. Boardwalk = \$2.2M



OPERATIONS AND MAINTENANCE



PRELIMINARY CONSTRUCTION COST ESTIMATE
Onalaska Parks Department(ONALA)
Great River Landing

Maintenance Costs

Updated

01/26/15

No.	Description	Units	Quantity	Unit Price	Total Cost
1	Daily Facility Check Labor & Trash Removal	Hrs	370	\$25.00	\$9,250.00
2	Periodic Maintenance Labor	Hrs	50	\$25.00	\$1,250.00
3	Landscaping Maintenance (Lawn & Garden)	Hrs	48	\$25.00	\$1,200.00
5	Building Cleaning (year-round facility)	Hrs	100	\$30.00	\$3,000.00
6	Snow Removal (assume 16 week window)	Hrs	30	\$50.00	\$1,500.00
6	Equipment Repairs	\$/Year	1	\$2,500.00	\$2,500.00
Subtotal					\$18,700.00
Contingencies					\$4,300.00
Project Total					<u>\$23,000.00</u>
Annual Operational & Maintenance Cost					<u>\$23,000.00</u>

Project Assumptions

Data Input

Time Period

1

Yrs

Daily Facility Check Labor & Trash Removal

1

Hrs/day

Periodic Maintenance Labor

1

Hrs/Week

Landscaping Maintenance (Lawn & Garden)

4

Hrs/Week

Building Cleaning (year-round facility)

2

Hr/week

Snow Removal (assume 16 week window)

2

Hrs/Week

Equipment Repairs

\$2,500.00

\$/Year

ACTION PLAN – NEXT STEPS

This document illustrates the holistic vision for the Great River Landing project as developed during the three day charrette and validated during the 30% document phase. In this validation, a series of four project “bundles” were highlighted with a detailed phasing approach to implement Project One which was identified by the public, Waterfront Committee and Common Council as the preferred first phase of construction. The following actions are recommended as the next steps to implement this project.

1. It is recommended that this report be approved by the Waterfront Committee and Common Council to guide the phased implementation of the Great River Landing project.
2. City staff will need to outline a funding strategy for Project 1.0 as a minimal first, catalytic phase of construction to fulfill obligations with the Wisconsin DNR and to gain momentum for subsequent phases. Operations and maintenance should be factored into this effort.
3. Construction documents should be developed for Project 1.0 so that the project can bid as soon as construction funding is available.
4. The stated desire of the Waterfront Committee has been to construct as much of Project One as possible as soon as possible and at the level of quality illustrated in the Schematic Design and 30% documents. As such, staff should begin seeking additional funding for the remainder of Project One as a first priority, as well as other phases should a near term funding match materialize. A detailed summary of potential funding sources to be considered for this project have been included in the appendix of this document and includes:
 1. Community Development Block Grants (CDBG)
 2. Community Development Investment Grants (CDIG)
 3. Knowles – Nelson Stewardship Program
 4. Recreational Boating Facilities Grants
 5. Sports Fish Restoration (SFR)
 6. Transportation Alternatives Program (TAP)
 7. Urban Nonpoint Source and Storm Water Management Grant Program (UNPS & SW)
 8. Freight Railroad Infrastructure Improvements Program (FRIIP)
 9. Burlington Northern Santa Fe Railway (BNSF)
 10. Tax Increment Financing (TIF)
 11. Private and corporate partnership opportunities
5. Additional phases of the Great River Landing project should proceed into design and construction phases as the City secures additional funding to implement these project.



Thank You



A

30% ENGINEERING
DRAWINGS

CITY OF ONALASKA, WISCONSIN

30% ENGINEERING PLANS FOR GREAT RIVER LANDING WATERFRONT IMPROVEMENTS

NOTE:
THE EXACT LOCATION OF UNDERGROUND UTILITIES SUCH AS GAS, TELEPHONE, FIBER OPTIC, ELECTRIC, CABLE TV, AND PIPE LINES ARE UNKNOWN. THE CONTRACTOR SHALL CONTACT DIGGERS HOTLINE AT 811 AND ALL OTHER UTILITY OWNERS WHICH ARE IN THE PROJECT LIMITS, BEFORE COMMENCING EXCAVATION

THE SUBSURFACE UTILITY QUALITY INFORMATION IN THIS PLAN IS LEVEL D. THIS UTILITY QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02 ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."



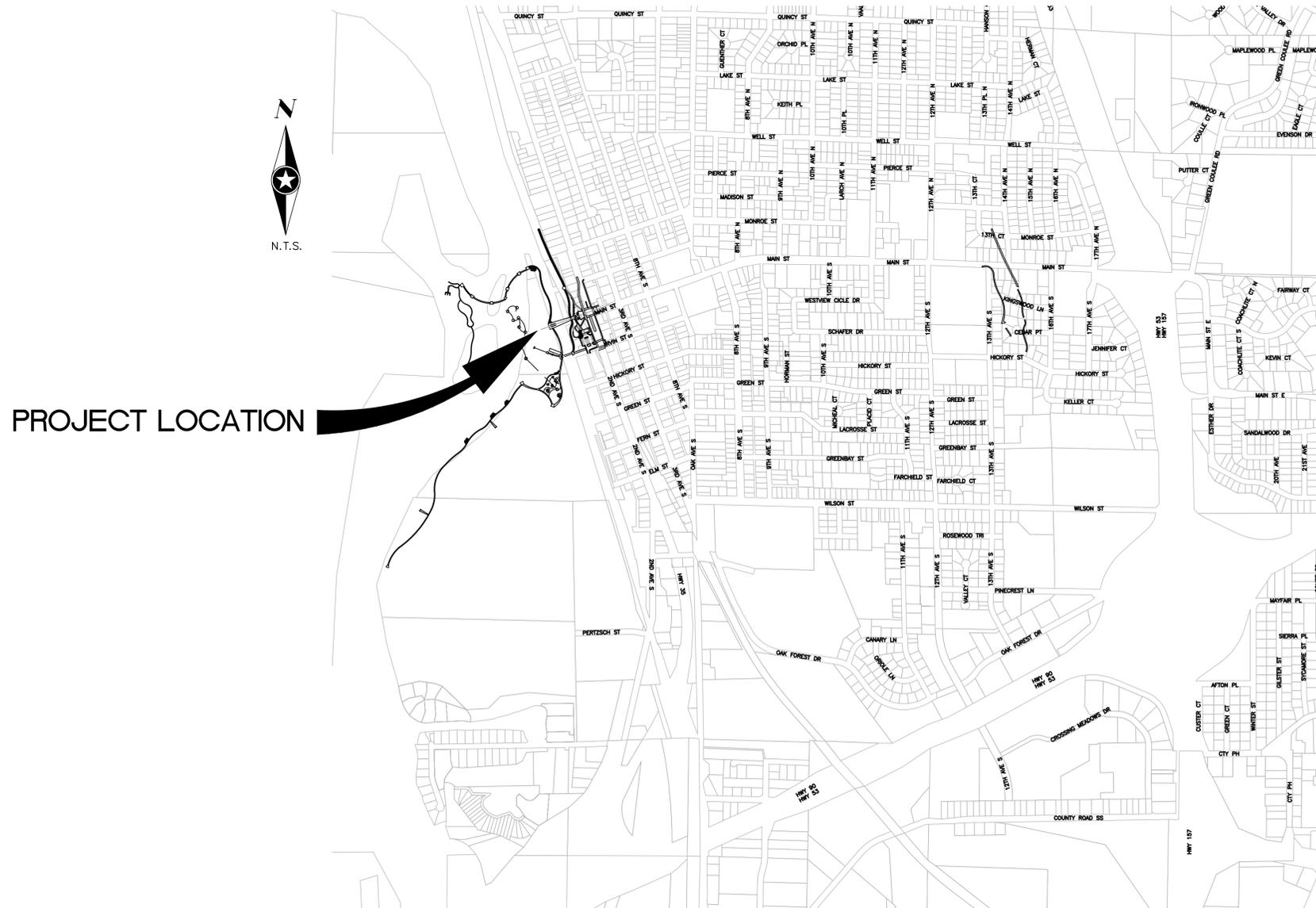
Know what's below.
Call before you dig.



GREAT RIVER LANDING
30% ENGINEERING PLANS
ONALASKA, WISCONSIN

INDEX

SHEET NO.	DESCRIPTION
1	TITLE
2	SITE DETAILS
3	KEY PLAN
4	EXISTING CONDITIONS PLAN
5	LAYOUT & MATERIALS PLAN
6	GRADING PLAN
7	UTILITIES PLAN
8	PEDESTRIAN BRIDGE PLAN & PROFILE
9	ARCHITECTURAL PLAN



PROJECT LOCATION

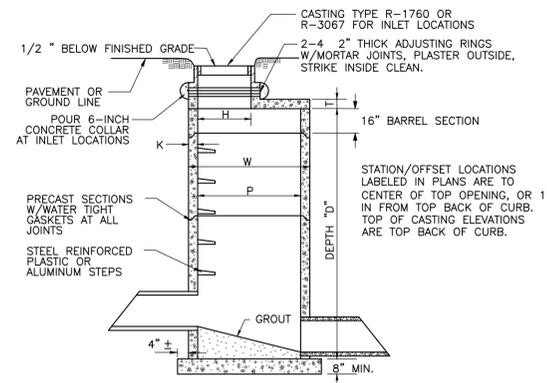
SEH FILE NO. ONALA 129196
PROJECT NO. 1-15-2015
ISSUE DATE J. RUBLE
DESIGNED BY B. CUNNINGHAM
DRAWN BY Short Elliott Hendrickson, Inc. (SEH)®

SHEET TITLE
TITLE SHEET

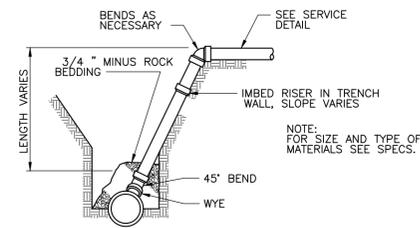
SHEET
1

MARK DATE DESCRIPTION REVISIONS

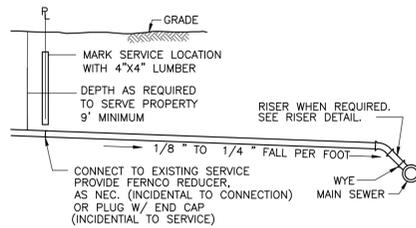
DIMENSIONS	MANHOLE TYPE					
	B	C	D	E	F	G
P	48"	60"	72"	84"	96"	102"
K	5"	6"	7"	8"	9"	9"
T	8"	8"	8"	8"	10"	10"
W	58"	72"	86"	100"	113"	119"
H	27"	27"	27"	27"	27"	27"



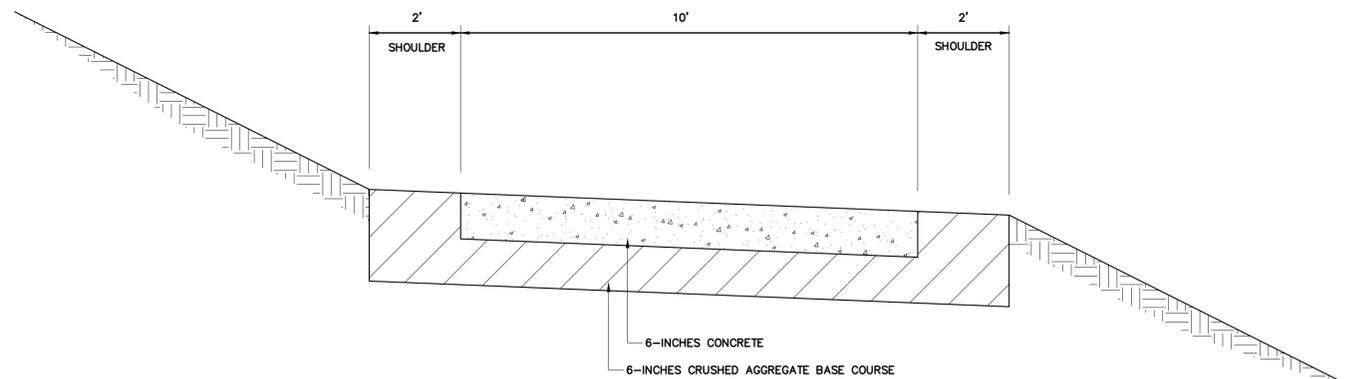
STORM MANHOLE



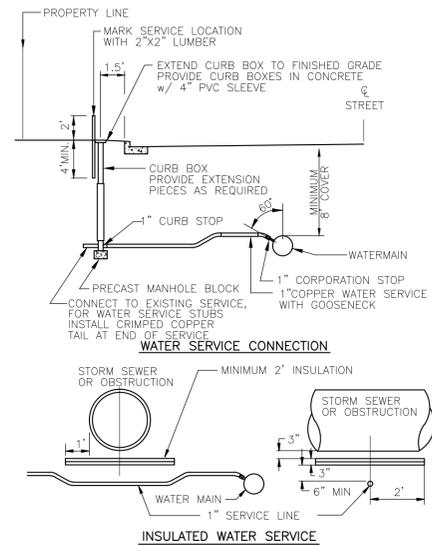
SANITARY SEWER SERVICE RISER



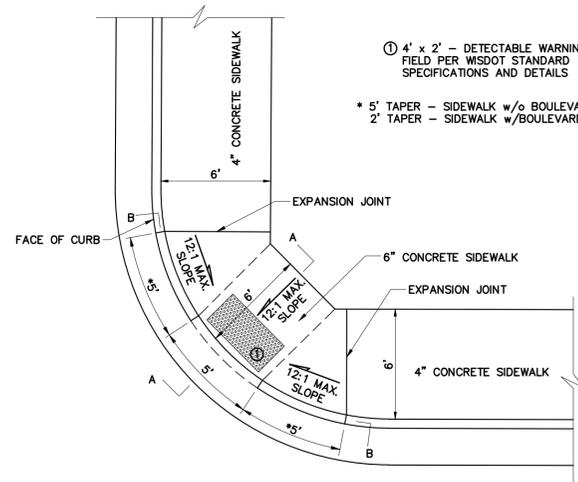
SANITARY SEWER SERVICE



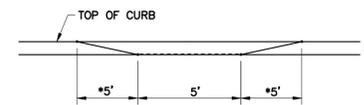
TYPICAL SECTION - CONCRETE TRAIL



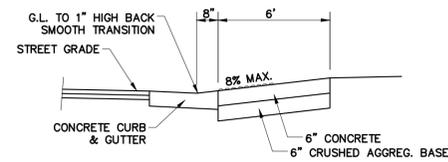
1" WATER SERVICE CONNECTION



PLAN

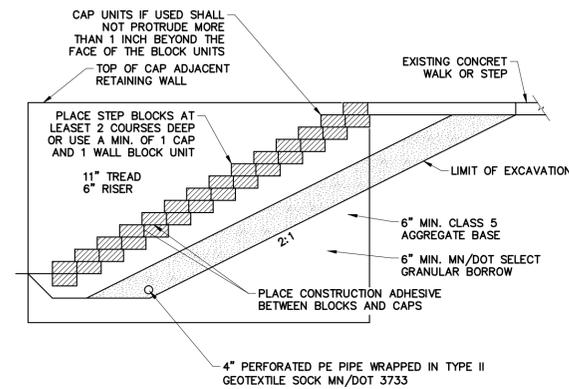


SECTION B-B

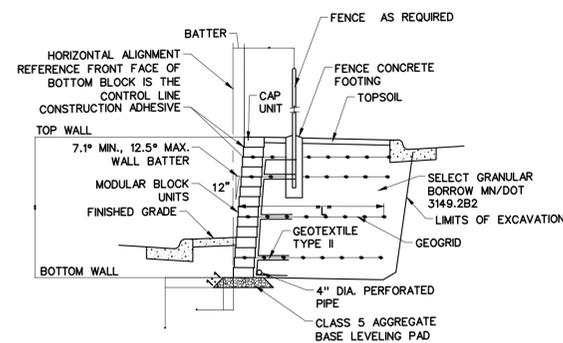


SECTION A-A

PEDESTRIAN RAMPS



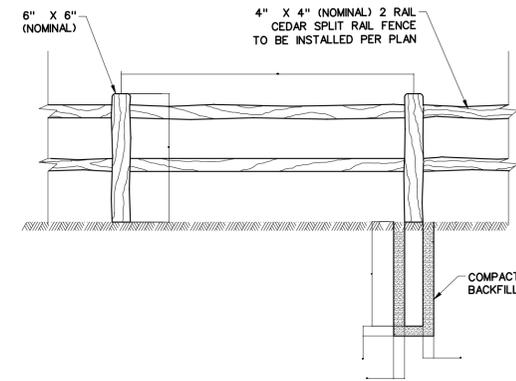
MODULAR BLOCK WALL TYPICAL STEP



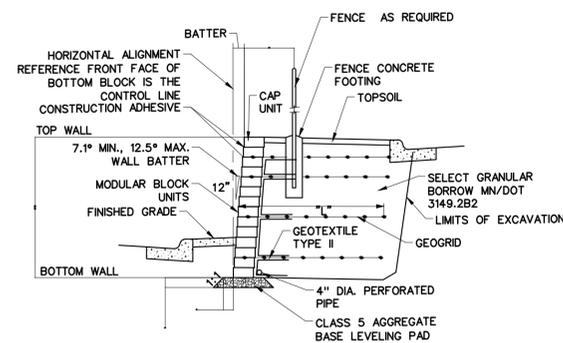
NOTES:

- GEOGRID LENGTH, "L", SHALL BE MEASURED FROM THE FRONT OF WALL AND IS SHOWN ON WALL PLAN AND PROFILE SHEETS.
- GEOGRID VERTICAL SPACING IS SHOWN ON WALL PLAN AND PROFILE SHEETS.
- BLOCKS SHALL HAVE A NOMINAL DEPTH OF 12".
- MODULAR BLOCK UNITS MUST HAVE INTERLOCKING OR LIP CONNECTIONS.
- FILL ALL VOID AREAS IN MODULAR BLOCK UNITS WITH COARSE FILTER AGGREGATE (Mn/DOT SPEC. 3149.2H) OR PEA GRAVEL AS APPROVED BY ENGINEER.
- RETAINING WALL LOCATION SHOWN ON PLANS.
- CONTRACTOR TO PROVIDE SHOP DRAWINGS FOR PROPOSED MODULAR BLOCK UNITS, (FOR GEOMETRY ONLY).
- FENCING POSTHOLES IN GEOGRID AREAS ARE TO BE AUGERED 12" MAX. CUT GEOGRID AS NEEDED TO INSTALL POSTS. DO NOT CUT PERPENDICULAR STRANDS.

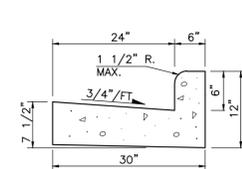
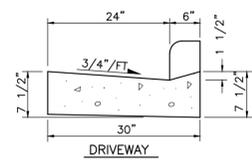
CONCRETE MODULAR BLOCK RETAINING WALL



SPLIT RAIL WOOD FENCE



BOLLARD DETAIL



NOTES:

THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE COURSE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED

TYPE D CURB & GUTTER



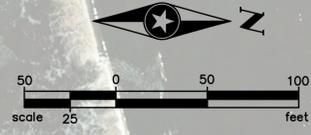
GREAT RIVER LANDING
30% ENGINEERING PLANS
ONALASKA, WISCONSIN

MARK	DATE	DESCRIPTION

SEH FILE NO.	ONALA 129196
PROJECT NO.	1-15-2015
ISSUE DATE	J. RUBLE
DESIGNED BY	B. CUNNINGHAM
DRAWN BY	Short Elliott Hendrickson, Inc. (SEH)®

SHEET TITLE
KEY PLAN

NOTE: AERIAL PHOTO REFLECTS CONDITIONS PRIOR TO STH 35 RECONSTRUCTION. ROADWAY MODIFICATIONS AND BUILDING REMOVALS HAVE SINCE OCCURED.



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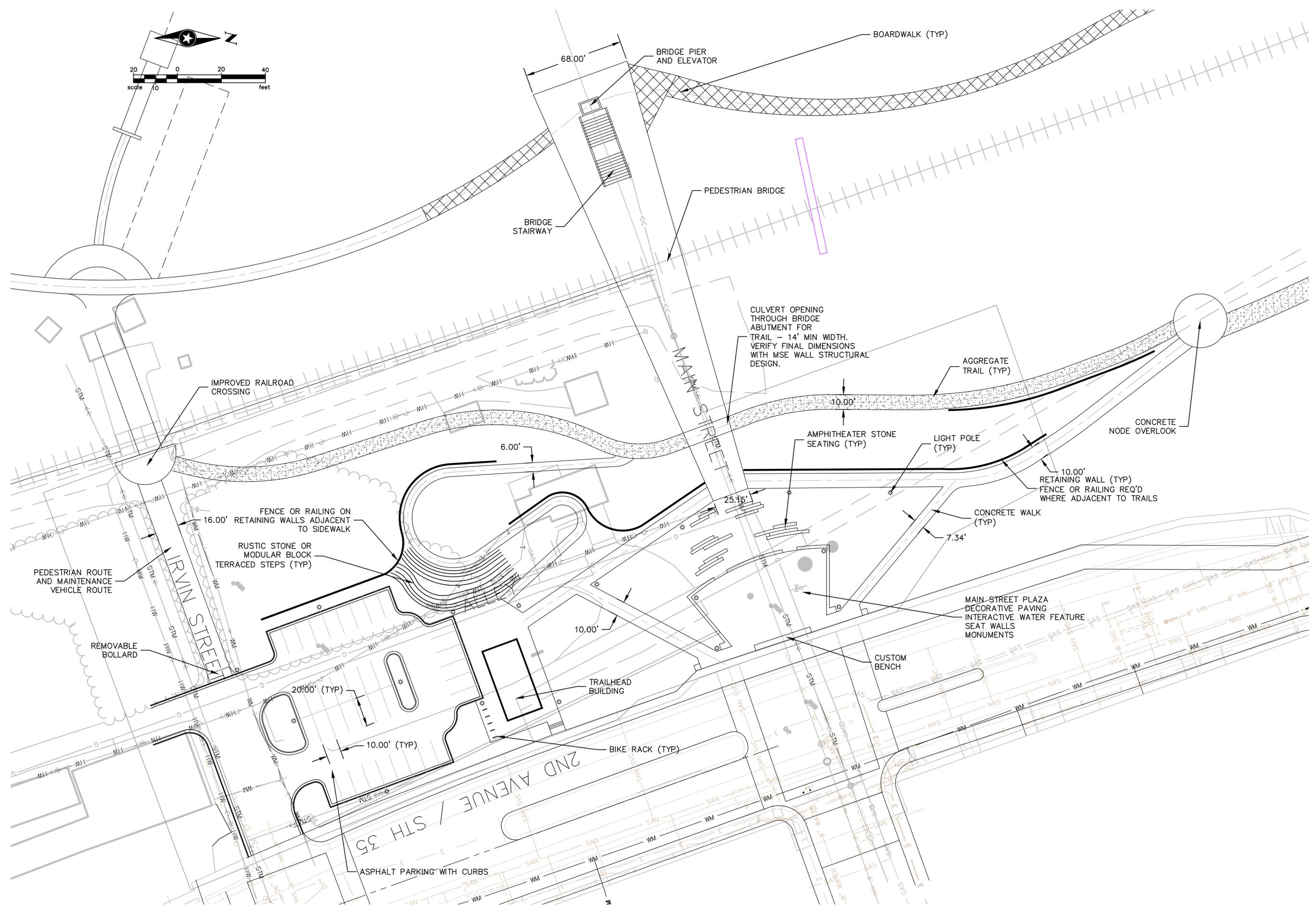
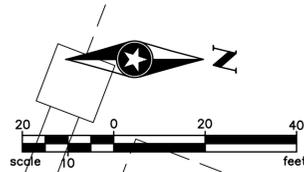
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 DRAWN BY B. CUNNINGHAM
 Short Elliott Hendrickson, Inc. (SEH)®

SHEET TITLE
**EXISTING
 CONDITIONS**

SHEET
4



P:\KO\ONALA\129196\C3D\Plansheets\on129196_Enlargement_Plan.dwg

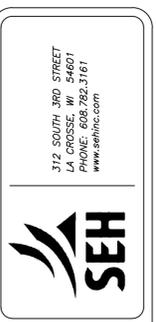
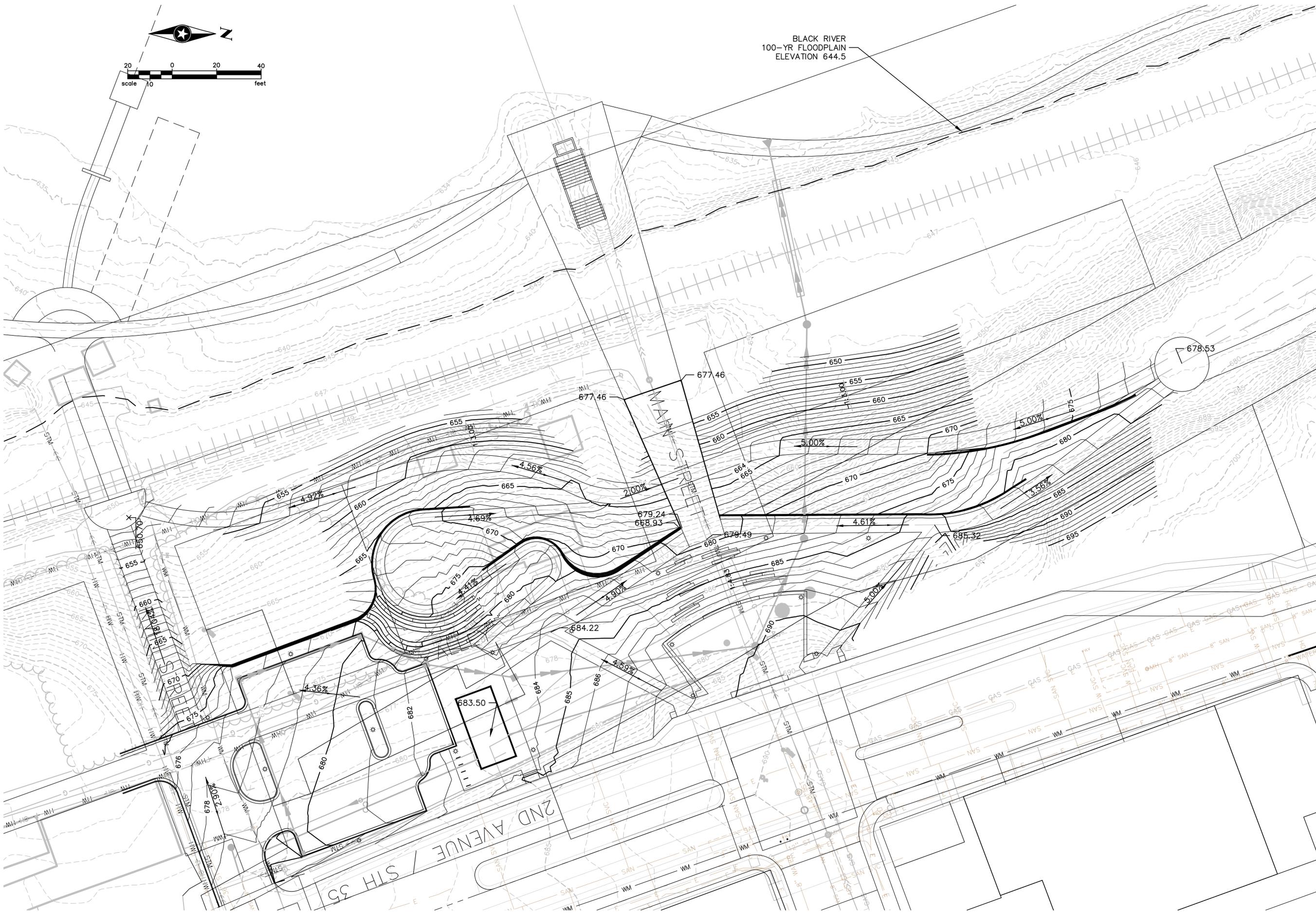


GREAT RIVER LANDING
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ONALASKA, WISCONSIN

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SHEET TITLE
LAYOUT AND MATERIALS PLAN

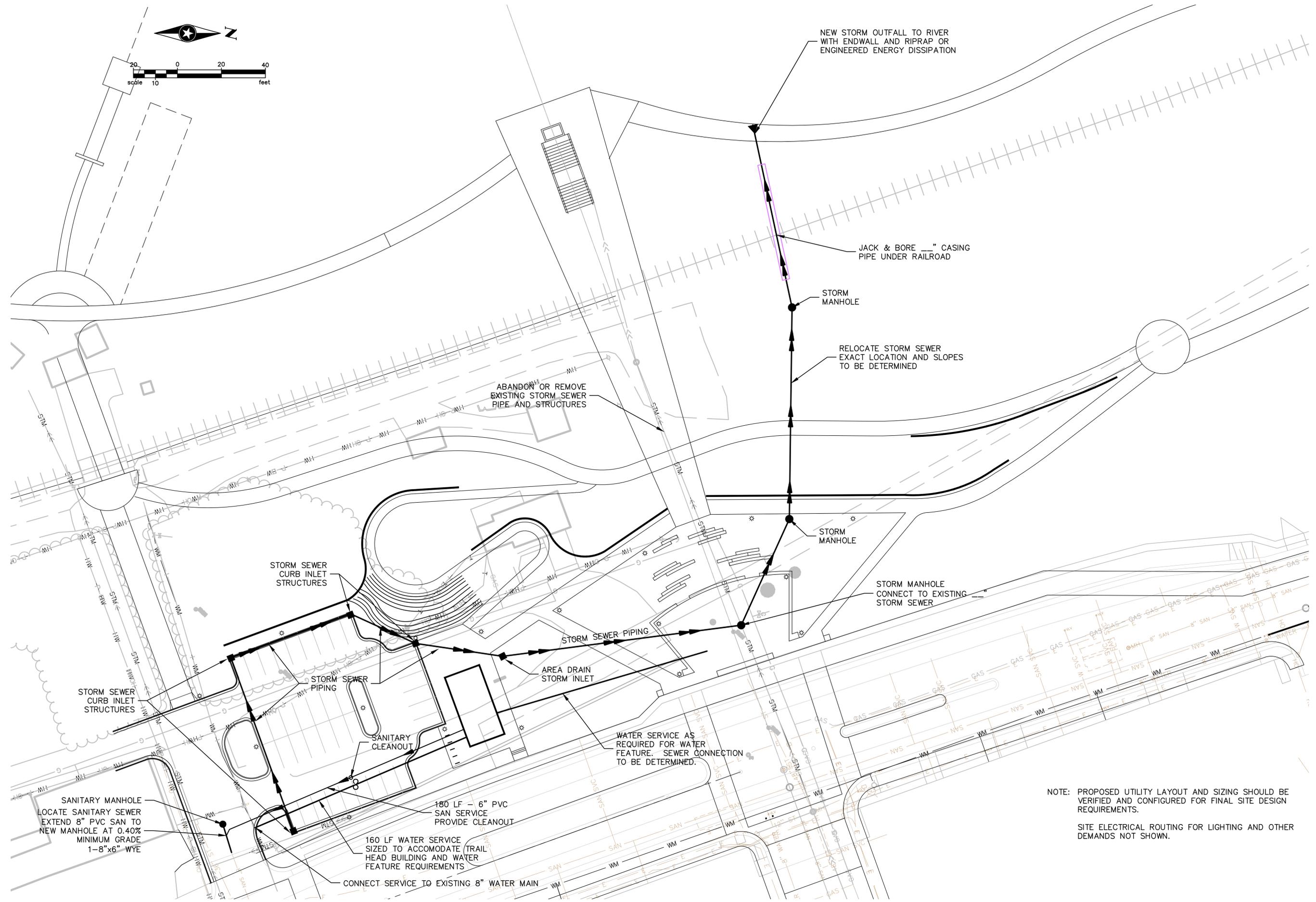
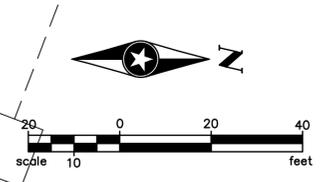


GREAT RIVER LANDING
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SHEET TITLE
GRADING PLAN



NEW STORM OUTFALL TO RIVER
WITH ENDWALL AND RIPRAP OR
ENGINEERED ENERGY DISSIPATION

JACK & BORE 12" CASING
PIPE UNDER RAILROAD

STORM
MANHOLE

RELOCATE STORM SEWER
EXACT LOCATION AND SLOPES
TO BE DETERMINED

ABANDON OR REMOVE
EXISTING STORM SEWER
PIPE AND STRUCTURES

STORM
MANHOLE

STORM MANHOLE
CONNECT TO EXISTING
STORM SEWER

STORM SEWER
CURB INLET
STRUCTURES

STORM SEWER
CURB INLET
STRUCTURES

STORM SEWER
PIPING

STORM SEWER PIPING

AREA DRAIN
STORM INLET

WATER SERVICE AS
REQUIRED FOR WATER
FEATURE. SEWER CONNECTION
TO BE DETERMINED.

180 LF - 6" PVC
SAN SERVICE
PROVIDE CLEANOUT

160 LF WATER SERVICE
SIZED TO ACCOMMODATE TRAIL
HEAD BUILDING AND WATER
FEATURE REQUIREMENTS

CONNECT SERVICE TO EXISTING 8" WATER MAIN

SANITARY MANHOLE
LOCATE SANITARY SEWER
EXTEND 8" PVC SAN TO
NEW MANHOLE AT 0.40%
MINIMUM GRADE
1-8"x6" WYE

NOTE: PROPOSED UTILITY LAYOUT AND SIZING SHOULD BE
VERIFIED AND CONFIGURED FOR FINAL SITE DESIGN
REQUIREMENTS.

SITE ELECTRICAL ROUTING FOR LIGHTING AND OTHER
DEMANDS NOT SHOWN.

P:\KO\ONALA\129196\C3D\Plansheets\on129196_Utility_Plan.dwg

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ONALASKA, WISCONSIN

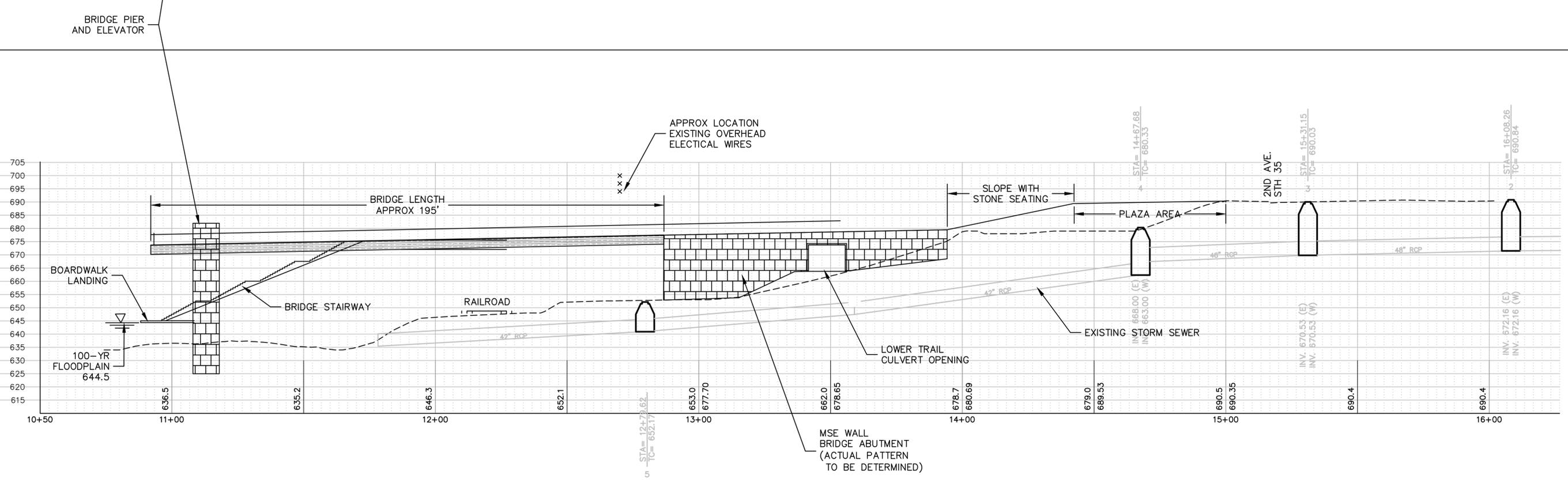
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PROJECT NO. 1-15-2015
ISSUE DATE J. RUBLE
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SHEET TITLE
UTILITIES PLAN

SHEET
7

P:\KOVO\ONALA\129196\C3D\Plansheets\on129196_Bridge_PP.dwg

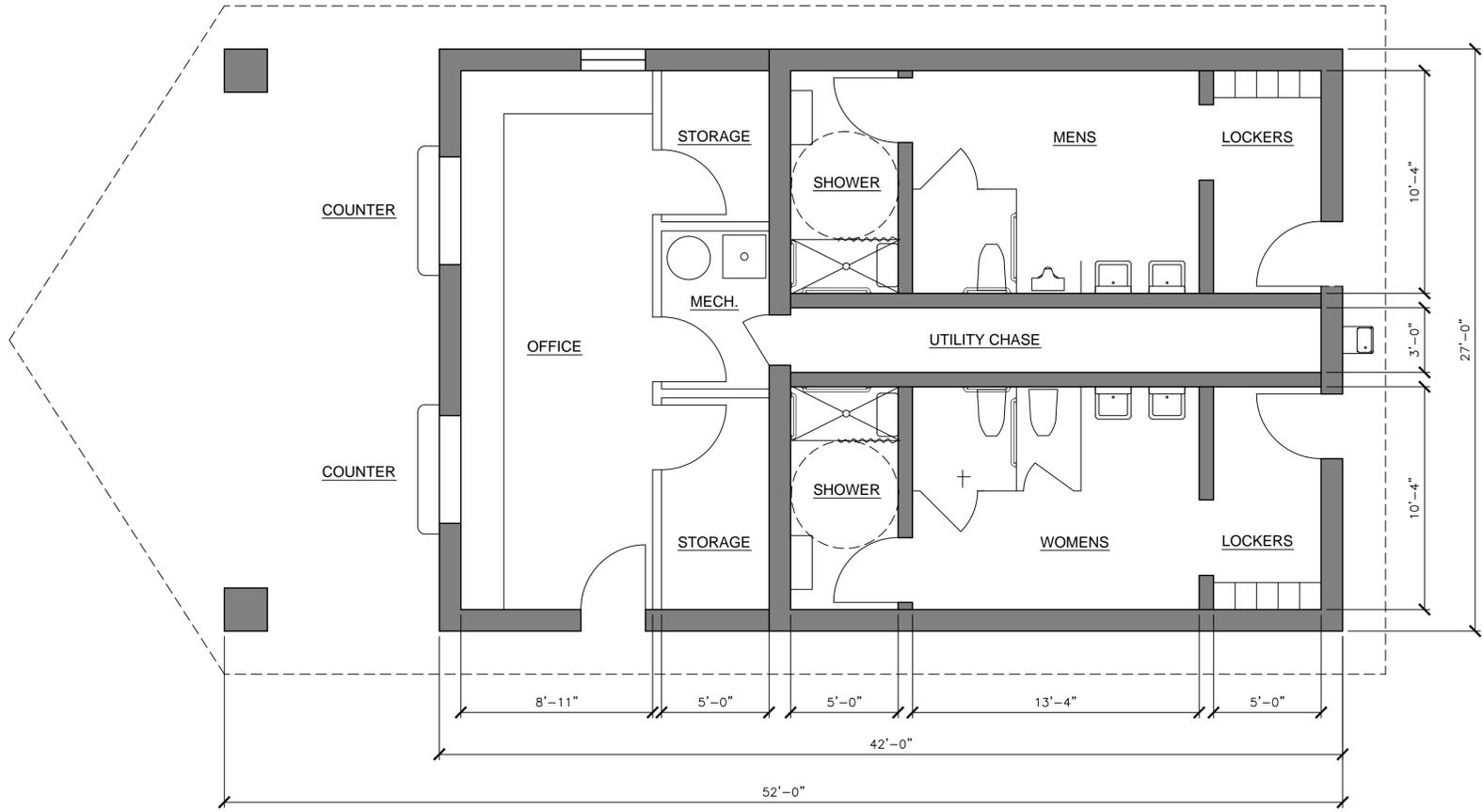


GREAT RIVER LANDING
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ONALASKA, WISCONSIN

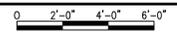
MARK	DATE	DESCRIPTION	REVISIONS

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PROJECT NO. 1-15-2015
ISSUE DATE J. RUBLE
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SHEET TITLE
**PEDESTRIAN BRIDGE
PLAN AND PROFILE**



1 PROPOSED SHELTER PLAN



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ONALASKA, WISCONSIN

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PROJECT NO. 1-15-2015
ISSUE DATE J. RUBLE
DESIGNED BY B. CUNNINGHAM
DRAWN BY
Shurt Elliott Hendrickson, Inc. (SEH)

SHEET TITLE
Trailhead Architectural Plans

SHEET
10

B

OPINION OF PROBABLE CONSTRUCTION COST



Date: 12/11/2014
SEH: ONALA - 129196

BUILDING THE GREAT RIVER LANDING
ONALASKA, WISCONSIN
PRELIMINARY ENGINEER'S OPINION OF PROBABLE CONSTRUCTION COST

Item	Unit	Total Quantity	Unit Cost	Total Cost
BUNDLE 1: PADDLE BASIN & TRAIL				
CONTRACTOR MOBILIZATION	LS	1	\$150,000.00	\$ 150,000.00
DEMO & EROSION CONTROL	LS	1	\$15,000.00	\$ 15,000.00
EARTHWORK	LS	1	\$10,000.00	\$ 10,000.00
BOARDWALK (10' WIDE, 8' CLEAR: ELEVATED STRUCTURE)	SFT	6000	\$200.00	\$ 1,200,000.00
OVERLOOK	LS	1	\$15,000.00	\$ 15,000.00
CONCRETE WALK	SFT	12500	\$5.00	\$ 62,500.00
DROP OFF & EMERGENCY ACCESS	SFT	2350	\$8.00	\$ 18,800.00
TURF	SFT	7050	\$2.00	\$ 14,100.00
LOG RUN BOOM	LS	1	\$45,000.00	\$ 45,000.00
ADA KAYAK LAUNCH	LS	1	\$10,000.00	\$ 10,000.00
NATURE PLAYGROUND	LS	1	\$250,000.00	\$ 250,000.00
SHADE SHELTERS	LS	2	\$50,000.00	\$ 100,000.00
SUPPORT STRUCTURE	LS	1	\$200,000.00	\$ 200,000.00
LANDSCAPE	LS	1	\$25,000.00	\$ 25,000.00
SUBTOTAL				\$2,115,400.00
<i>Design-Construction Contingency (15%)</i>				<i>\$317,310.00</i>
TOTAL				\$2,432,710.00
BUNDLE 2: BOARDWALK & SPILLWAY LINK				
CONTRACTOR MOBILIZATION	LS	1	\$140,000.00	\$ 140,000.00
DEMO & EROSION CONTROL	LS	1	\$30,000.00	\$ 30,000.00
EARTHWORK	LS	1	\$10,000.00	\$ 10,000.00
BOARDWALK (10' WIDE, 8' CLEAR: ELEVATED STRUCTURE)	SFT	8000	\$200.00	\$ 1,600,000.00
STONE LEDGE WATER ACCESS	CYS	740	\$125.00	\$ 92,500.00
LANDSCAPE RESTORATION	LS	1	\$30,000.00	\$ 30,000.00
SUBTOTAL				\$1,902,500.00
<i>Design-Construction Contingency (15%)</i>				<i>\$285,375.00</i>
TOTAL				\$2,187,875.00
BUNDLE 3: SOUTH NATURE TRAIL ENHANCEMENTS				
CONTRACTOR MOBILIZATION	LS	1	\$35,000.00	\$ 35,000.00
DEMO & EROSION CONTROL	LS	1	\$40,000.00	\$ 40,000.00
EARTHWORK	LS	1	\$5,000.00	\$ 5,000.00
BOARDWALK (WOOD WITH TOE RAIL)	SFT	3200	\$80.00	\$ 256,000.00
SOFT TRAILS (AGGREGATE)	SFT	20500	\$3.00	\$ 61,500.00
LANDSCAPE RESTORATION	LS	1	\$35,000.00	\$ 35,000.00
SUBTOTAL				\$432,500.00
<i>Design-Construction Contingency (15%)</i>				<i>\$64,875.00</i>
TOTAL				\$497,375.00
BUNDLES PILLWAY BRIDGES				
CONTRACTOR MOBILIZATION	LS	1	\$200,000.00	\$ 200,000.00
DEMO & EROSION CONTROL	LS	1	\$50,000.00	\$ 50,000.00
EARTHWORK	LS	1	\$5,000.00	\$ 5,000.00
BOARDWALK (10' WIDE, 8' CLEAR: ELEVATED STRUCTURE)	SFT	13200	\$200.00	\$ 2,640,000.00
LANDSCAPE RESTORATION	LS	1	\$20,000.00	\$ 20,000.00
SUBTOTAL				\$2,915,000.00
<i>Design-Construction Contingency (15%)</i>				<i>\$437,250.00</i>
TOTAL				\$3,352,250.00



Date: 12/11/2014
SEH: ONALA - 129196

BUILDING THE GREAT RIVER LANDING
ONALASKA, WISCONSIN
PRELIMINARY ENGINEER'S OPINION OF PROBABLE CONSTRUCTION COST

Item	Unit	Total Quantity	Unit Cost	Total Cost
ADVENTURE PLAY & ISLAND RECREATION				
CONTRACTOR MOBILIZATION	LS	1	\$40,000.00	\$ 40,000.00
DEMO & EROSION CONTROL	LS	1	\$25,000.00	\$ 25,000.00
EARTHWORK	LS	1	\$30,000.00	\$ 30,000.00
SOFT TRAILS (AGGREGATE)	SFT	7650	\$3.00	\$ 22,950.00
TRANSIENT DOCKS AND GANGWAY	LS	1	\$70,000.00	\$ 70,000.00
ZIP LINES & HIGH ROPES	LS	1	\$300,000.00	\$ 300,000.00
LANDSCAPE	LS	1	\$50,000.00	\$ 50,000.00
SUBTOTAL				\$537,950.00
<i>Design-Construction Contingency (15%)</i>				<i>\$134,487.50</i>
TOTAL				\$672,437.50

TRAILHEAD & MARKET				
CONTRACTOR MOBILIZATION	LS	1	\$55,000.00	\$ 55,000.00
DEMO & EROSION CONTROL	LS	1	\$20,000.00	\$ 20,000.00
EARTHWORK (EXCAVATION)	CYS	600	\$7.00	\$ 4,200.00
EARTHWORK (FILL)	CYS	5500	\$12.00	\$ 66,000.00
SANITARY SEWER SERVICE PIPING AND CONNECTIONS	LS	1	\$5,500.00	\$ 5,500.00
SANITARY SEWER EXTENSION IN IRVIN	LS	1	\$4,500.00	\$ 4,500.00
WATER SERVICE PIPING AND CONNECTION	LS	1	\$6,500.00	\$ 6,500.00
RETAINING WALL (CAST CONCRETE)	SFT	2750	\$35.00	\$ 96,250.00
RAILING (FOR RETAINING WALLS)	LFT	300	\$120.00	\$ 36,000.00
TRAILHEAD BUILDING	LS	1	\$250,000.00	\$ 250,000.00
ASPHALT PARKING	SFT	16070	\$5.00	\$ 80,350.00
ASPHALT REMOVAL (IRVIN ROAD DIET)	SFT	2350	\$3.00	\$ 7,050.00
RAILROAD CROSSING	LS	1	\$125,000.00	\$ 125,000.00
PARKING CURB	LFT	720	\$20.00	\$ 14,400.00
SHADE STRUCTURE	LS	1	\$100,000.00	\$ 100,000.00
CONCRETE WALK	SFT	3000	\$5.00	\$ 15,000.00
ASPHALT TRAIL	SFT	5100	\$4.00	\$ 20,400.00
AGGREGATE TRAIL	SFT	5660	\$3.00	\$ 16,980.00
RUSTIC STONE STEPS	LS	1	\$35,000.00	\$ 35,000.00
LIGHTING	LS	1	\$75,000.00	\$ 75,000.00
ELECTRICAL DISTRIBUTION	LS	1	\$25,000.00	\$ 25,000.00
TURF SOD	SYS	750	\$6.00	\$ 4,500.00
LANDSCAPE	LS	1	\$50,000.00	\$ 50,000.00
SUBTOTAL				\$1,112,630.00
<i>Design-Construction Contingency (15%)</i>				<i>\$166,894.50</i>
TOTAL				\$1,279,524.50



Date: 12/11/2014
SEH: ONALA - 129196

BUILDING THE GREAT RIVER LANDING
ONALASKA, WISCONSIN
PRELIMINARY ENGINEER'S OPINION OF PROBABLE CONSTRUCTION COST

Item	Unit	Total Quantity	Unit Cost	Total Cost
MAIN STREET BRIDGE				
CONTRACTOR MOBILIZATION	LS	1	\$185,000.00	\$ 185,000.00
RAISE OVERHEAD TRANSMISSION LINES	LS	1	\$325,000.00	\$ 325,000.00
DEMO & EROSION CONTROL	LS	1	\$40,000.00	\$ 40,000.00
EARTHWORK	LS	1	\$40,000.00	\$ 40,000.00
BRIDGE	SFT	7400	\$210.00	\$ 1,554,000.00
BRIDGE ELECTRICAL DISTRIBUTION	LS	1	\$50,000.00	\$ 50,000.00
LIGHTING	LS	1	\$50,000.00	\$ 50,000.00
STAIRWAY	LS	1	\$80,000.00	\$ 80,000.00
ELEVATOR	LS	1	\$300,000.00	\$ 300,000.00
LANDSCAPE RESTORATION	LS	1	\$10,000.00	\$ 10,000.00

SUBTOTAL	\$2,634,000.00
<i>Design-Construction Contingency (15%)</i>	\$395,100.00
TOTAL	\$3,029,100.00

MAIN STREET PLAZA, SPRAY & LAWN				
CONTRACTOR MOBILIZATION	LS	1	\$90,000.00	\$ 90,000.00
DEMO & EROSION CONTROL	LS	1	\$50,000.00	\$ 50,000.00
STORM SEWER RELOCATION (42" PIPE)	LFT	250	\$125.00	\$ 31,250.00
STORM SEWER RELOCATION (MANHOLES)	EA	3	\$4,000.00	\$ 12,000.00
STORM SEWER RELOCATION (RAILROAD CROSSING)	LFT	100	\$400.00	\$ 40,000.00
STORM PIPE SITE DRAINAGE (12" - 18")	LFT	500	\$40.00	\$ 20,000.00
STORM SITE DRAINAGE STRUCTURES	EA	5	\$1,500.00	\$ 7,500.00
STORMWATER MGMT FEATURE	LS	1	\$10,000.00	\$ 10,000.00
EARTHWORK (EXCAVATION)	CYS	1250	\$7.00	\$ 8,750.00
EARTHWORK (FILL)	LS	1	\$60,000.00	\$ 60,000.00
BENCH	LFT	150	\$350.00	\$ 52,500.00
SPRAY FOUNTAIN & UTILITIES	LS	1	\$200,000.00	\$ 200,000.00
SPRAY FOUNTAIN PAVING	SFT	4375	\$12.00	\$ 52,500.00
RETAINING WALL (FOR TRAILS)	SFT	3850	\$35.00	\$ 134,750.00
RAILING (FOR RETAINING WALLS)	LFT	120	\$120.00	\$ 14,400.00
RETAINING WALL (FOR OVERLOOK/BRIDGE ABUTMENT)	SFT	5100	\$35.00	\$ 178,500.00
CULVERT (FOR LOWER TRAIL)	LS	1	\$60,000.00	\$ 60,000.00
RAILING (FOR OVERLOOK)	LFT	220	\$250.00	\$ 55,000.00
CONCRETE WALK	SFT	13000	\$8.00	\$ 104,000.00
CONCRETE STEPS	SFT	4900	\$55.00	\$ 269,500.00
TURF SOD	SYS	1500	\$6.00	\$ 9,000.00
LANDSCAPE	LS	1	\$75,000.00	\$ 75,000.00

SUBTOTAL	\$1,534,650.00
<i>Design-Construction Contingency (15%)</i>	\$230,197.50
TOTAL	\$1,764,847.50

OVERALL CONSTRUCTION TOTAL	\$15,216,119.50
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FUNDING
MEMORANDUM



Building a Better World
for All of Us®

MEMORANDUM

TO: Brea Grace & Jarrod Holter

FROM: SEH

DATE: November 5, 2014

RE: Funding Sources for the Great River Landing
SEH No. ONALA 129196 14.00

Section 1: Program Descriptions

Community Development Block Grants (CDBG)

Administered by the Wisconsin Department of Administration, the CDBG Program aims to assist provide funding assistance for projects that will benefit low to moderate income families. There are several population segments that are considered LMI automatically, including handicapped, seniors, etc.

Deadline: June 2015

Funding Available: Up to 50% of project costs (\$500,000 maximum).

Community Development Investment Grants (CDIG)

Offered by the Wisconsin Economic Development Corporation (WEDC), the CDIG Program offers financial assistance with projects that will have a direct economic benefit to downtowns.

Deadline: January and April, 2015

Funding Available: Up to 25% of project costs (\$500,000 maximum).

Requirements: Must be "shovel-ready."

Knowles – Nelson Stewardship Program

Offered by the Wisconsin Department of Natural Resources (WDNR), this program provides funding for outdoor, nature based recreation improvements.

*Applying for this also allows your project to be scored and ranked for the following federal programs:

- Urban Green Space grants
- Urban Rivers grants
- Land and Water Conservation Fund
- Recreational Trails Act (RTA)

Deadline: May 1st, annually

Funding Available: Up to 50% of project costs

Requirements: Must have an up-to-date adopted Outdoor Park and Recreation Plan with the specific projects identified in the plan.

Engineers | Architects | Planners | Scientists

Short Elliott Hendrickson Inc., 312 South 3rd Street, La Crosse, WI 54601-4007

SEH is 100% employee-owned | sehinc.com | 608.782.3161 | 888.908.8166 fax



Recreational Boating Facilities Grants

Offered by the Wisconsin Department of Natural Resources (WDNR), this program can be used for dredging, ramps/boardwalk docks, harbors of refuge, support facilities, and trash skimming and weed harvesting equipment.

Deadline: Year-Round; Applications are reviewed quarterly.

Funding Available: Up to 50% of project costs.

Sports Fish Restoration (SFR)

This program, offered by the Wisconsin Department of Natural Resources (WDNR), provides funding to construct fishing piers and motorboat access projects, including boat ramps and related amenities, such as parking lots, accessible paths, lighting and restroom facilities.

Deadline: Year-Round; however, February 1 for consideration in federal fiscal year that begins each October.

Transportation Alternatives Program (TAP)

The TAP Program is offered by the Wisconsin Department of Transportation (WisDOT) and replaces three former programs. This will fund the planning, design and construction of on and off-road trail facilities and safe routes to school activities.

Deadline: Anticipated funding cycle in the fall of 2015.

Funding Available: Up to 80% of project costs.

Urban Nonpoint Source and Storm Water Management Grant Program (UNPS & SW)

Offered by the Wisconsin Department of Natural Resources (WDNR), the UNPS and SW Program will provide funding assistance for the construction of urban best management practices (BMPs) including detention and infiltration measures, storm sewer improvements, streambank and shoreline stabilization and land acquisition.

Deadline: April 15, 2015.

Funding Available: Up to 50% of project costs (\$200,000 maximum).

Freight Railroad Infrastructure Improvement Program (FRIIP)

The Wisconsin Department of Transportation offers funds for projects that, among other elements, make improvements to enhance transportation efficiency, safety, and intermodal freight movement, as well as develop the economy.

Deadline: February 2015.

Funding Available: 100% Loan Funds

Burlington Northern Santa Fe Railway

An applicant organization or program must be in a community near one of BNSF's rail lines. Eligible activities include civic services--including organizations concerned with the environment and local issues such as parks and recreation.

Deadline: Year-Round. The Foundation's Grants Review Team meets monthly, but decisions can take 3 to 12 months.

Funding Available: Generally between \$1,000 and \$10,000

Tax Increment Financing

TIF District's pledge current increased tax revenues due to increased values to construct projects that will aid in the development or redevelopment of a defined District. The City has one existing TIF District and has significant capacity to create additional Districts if desired, and if anticipated that additional valuation will be developed in the downtown area. TIF Districts cannot include wetland areas, and as such, funds

cannot generally be spent on projects in wetlands or surface waters, which will limit the projects that TIF could be used for. Funding from the City's current TIF District would be based on the amount of increment the district has gained since inception. Project funding would need to be consistent with the TIF Project Plan.

Section 2: Project Program Elements – Funding Opportunities

The following section identifies potential funding sources for the identified program elements. Grant funding is never guaranteed, but these programs are the most likely to fund the identified programmatic elements. Careful wording and structuring of the applications will need to be done in order to “sell” the projects to the funding agencies. All agencies are interested in “purchasing” solutions to certain problems or challenges, so structuring the applications to best cater to the funding agencies interests is key.

- 1. Transient Boat Docking**
 - Recreational Boating Facilities Grants
 - Sports Fish Restoration
 - TIF*
- 2. Ice Fishing Bay**
- 3. Spillway Island Bridging**
 - Knowles-Nelson Stewardship
 - Transportation Alternatives Program (TAP) (via bicycle and pedestrian trail connections)
- 4. Nature Walk & Rustic Camping**
 - Knowles-Nelson Stewardship
 - Transportation Alternatives Program (TAP) (via bicycle and pedestrian trail connections)
 - TIF*
- 5. Aerial Canopy Course**
 - TIF*
- 6. Spillway Fishing Pier**
 - Knowles-Nelson Stewardship
 - Sports Fish Restoration
- 7. Pedestrian Bridge with Grand Stairway & Elevator**
 - Knowles-Nelson Stewardship
 - Sports Fish Restoration
 - Freight Rail Infrastructure Improvement Program
 - Burlington Northern Santa Fe
 - Urban Nonpoint Source & Storm Water Management
 - TIF*
- 8. Main Street Plaza**
 - Community Development Investment Grant (CDIG)
 - Urban Nonpoint Source & Storm Water Management
 - TIF*
- 9. Market, Trailhead & Restroom Facility**
 - Community Development Investment Grant (CDIG)
 - Knowles-Nelson Stewardship
 - Sports Fish Restoration
 - Urban Nonpoint Source & Storm Water Management
 - TIF*
- 10. Enhanced Pedestrian & Emergency Railroad Crossing**
 - Knowles-Nelson Stewardship
 - Transportation Alternatives Program (TAP) (via bicycle and pedestrian trail connections)
 - Freight Rail Infrastructure Improvement Program
 - Burlington Northern Santa Fe
 - TIF*

11. **Emergency Boat Launch, ADA Kayak Launch & Log Rolling Boom**
 - Community Development Block Grant (CDBG) (for ADA Improvements)
 - Knowles-Nelson Stewardship
 - Sports Fish Restoration
 - TIF*
12. **Stone Seating Blocks**
 - TIF*
13. **Adventure Play Area**
 - TIF*
14. **Support Structure**
 - Knowles-Nelson Stewardship
 - TIF*
15. **Fabric Picnic Shelters**
 - Knowles-Nelson Stewardship
 - TIF*
16. **Stone River Access**
 - Knowles-Nelson Stewardship
 - Sports Fish Restoration
17. **Wetland Viewing Access**
 - Knowles-Nelson Stewardship
18. **Nature Walk**
 - Knowles-Nelson Stewardship
 - Transportation Alternatives Program (TAP) (via bicycle and pedestrian trail connections)
19. **Downtown Redevelopment** - Although not identified specifically as a program element, several downtown redevelopment opportunities were identified in the mapping, and as such, is reviewed and included for potential funding opportunities to help spur the private development.
 - Community Development Investment Grants (CDIG)
 - TIF*

*If consistent with TIF Project Plan.

Section 3: Phasing

As project priorities are developed, it will be important to consider phasing strategies, as many of the grant programs discussed have annual cycles. The City can certainly apply for multiple cycles for many of the programs, so to maximize the potential funding assistance, a phasing strategy that both keeps projects manageable and maximizes grant funding. For example, DNR through their Stewardship program would rather complete multiple projects spread over a series of years rather than one large project in a community.

APPENDIX

D

WETLAND REPORT

Legend

 Project Limits

 Wetland Line

 Upland Island

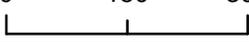
 Culvert

Sample Point Type

 upland

 wetland

0 150 300 Feet




Lake Onalaska

Black River

Upland

Wetland 1

Wetland 1

4w

3u

1u

2w

Path: \\sehia\projects\K0\ONALA\129196\3-Background_Research\Wetlands\Figures for report\Fig 5- Wetland.mxd



10 North Bridge Street
Chippewa Falls, WI 54729-2550
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Fax: 888.908.8166
www.sehinc.com

Project: ONALA 129196
Print Date: 2/2/2015

Map by: rbeduhn
Projection: NAD983_HARN
LaCrosse_County_Ft

Source: SEH, ESRI

Wetland Delineation Results

Waterfront Improvements Project
City of Onalaska, Wisconsin

Figure
5

This map is neither a legally recorded map nor a survey map and is not intended to be used as one. This map is a compilation of records, information, and data gathered from various sources listed on this map and is to be used for reference purposes only. SEH does not warrant that the Geographic Information System (GIS) Data used to prepare this map are error free, and SEH does not represent that the GIS Data can be used for navigational, tracking, or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features. The user of this map acknowledges that SEH shall not be liable for any damages which arise out of the user's access or use of data provided.

E

WATERFRONT COMMITTEE MEETING MINUTES

APPENDIX E

Waterfront Committee Meetings Minutes

Waterfront Committee Meetings

- August 20th, 2014
- September 11th, 2014
- September 29th, 2014
- September 30th, 2014
- November 3rd, 2014
- December 11th, 2014
- January 14th, 2015

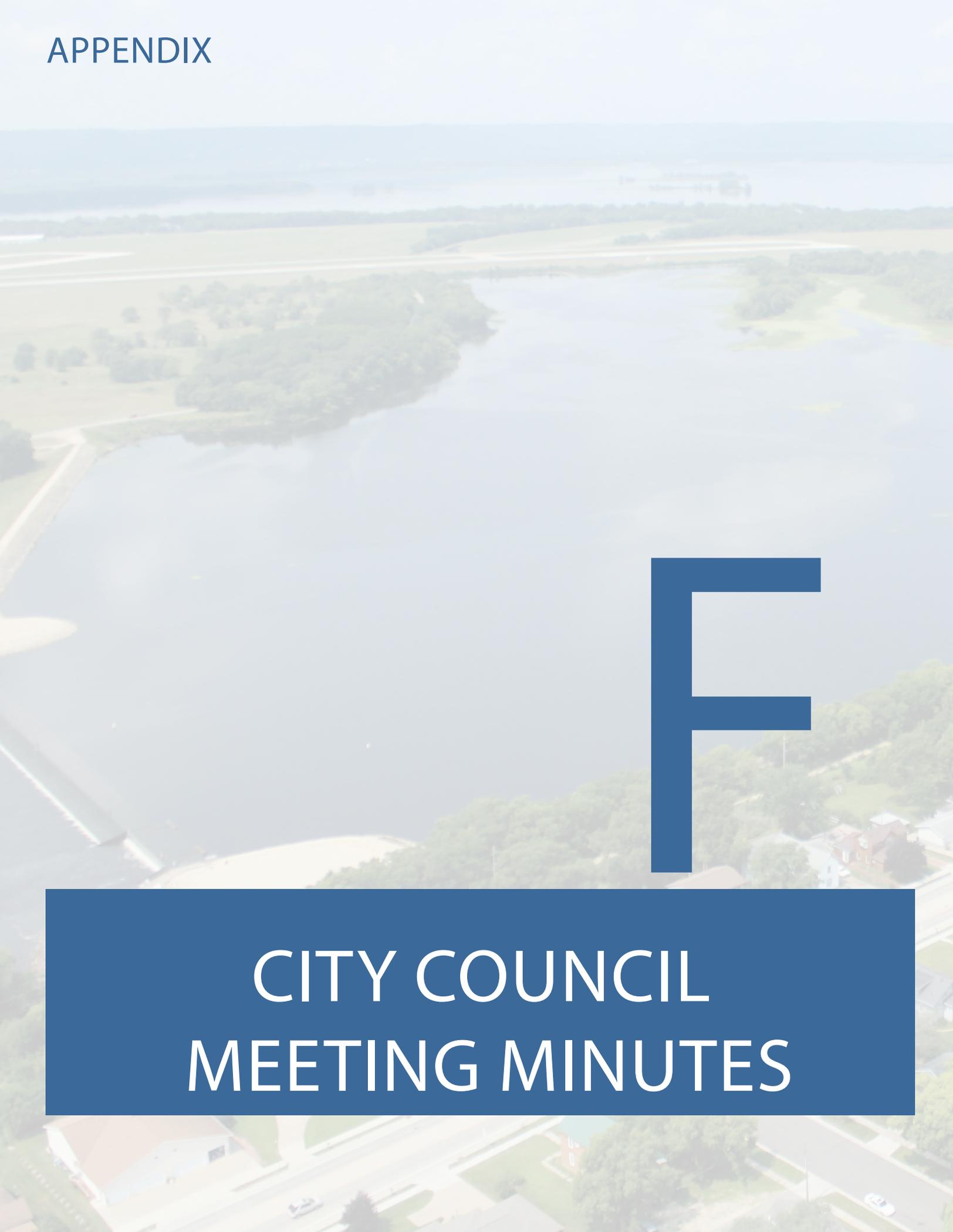
Community Meetings

- August 20th, 2014
- September 29th, 2014
- October 1st, 2014

Community Development Authority Meetings during the 3-Day Charrette

- August 20th, 2014

A copy of the meeting minutes may be found on the City's website at: www.cityofonalaska.com, under "Agendas & Minutes". Alternatively a copy may be requested from the Clerk's Office at City Hall, 415 Main Street, Onalaska, WI.

An aerial photograph of a large body of water, likely a reservoir or lake, with a dam on the left side. The water is a deep blue-grey color. The surrounding land is green with trees and some residential buildings. The sky is a pale, hazy blue.

F

CITY COUNCIL
MEETING MINUTES

APPENDIX F

City Council Meetings Minutes

- November 11th, 2014

A copy of the meeting minutes may be found on the City's website at: www.cityofonalaska.com, under "Agendas & Minutes". Alternatively a copy may be requested from the Clerk's Office at City Hall, 415 Main Street, Onalaska, WI.

